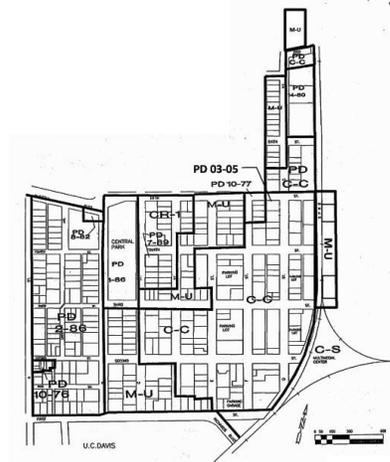


CORE AREA SPECIFIC PLAN



AS ADOPTED BY
Resolution 8021 by City Council on November 13, 1996

AND AS AMENDED BY
Resolution 8061 - March 12, 1997
Resolution 05-220 - July 12, 2005
Resolution 07-002 - January 9, 2007
Resolution 07-094 - June 12, 2007
Resolution 08-189 - December 16, 2008
Resolution 10-143 – September 21, 2010
Resolution 13-125 - August 27, 2013

Reprinted: September 2013



COMMUNITY DEVELOPMENT AND SUSTAINABILITY DEPARTMENT
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Forward

The origins of the Core Area Specific Plan can be traced back to 1961. At that time, the citizens of Davis realized that the Core Area was important enough to develop a plan that encouraged businesses and improved transportation patterns for the university oriented downtown. Time passes and conditions change but the basic ideas behind that original plan remain current.

Just as with the original plan the current Core Area plan addresses the vitality and economic health of the downtown. The issues present in 1961 remain surprisingly similar. Pedestrian, bicycle, and automobile circulation issues are increasingly important especially given that University enrollment continues to rise. Parking demand in the downtown remains a complex issue that affects and is affected by other modes of transportation. Increasing retail space in the downtown continues to be an elusive goal that affects development throughout the City. These issues and many others frame the discussion that began in 1961 and continues today.

The current effort to enhance the vitality of the Core Area and address the issues unique to this part of the city began in 1989. The City Council appointed a citizen task force to identify the problems and propose solutions. Their objective was to guide growth in the Core Area so that it remained the pedestrian-oriented economic and social center of the City. Through dedication and hard work the Task Force produced a consensus plan by 1990. Unresolved issues related to the scope of the Task Force's objective delayed the release of a draft plan until 1994.

The current Core Area Specific Plan contains updates based on new information that was not available to the Task Force at the time the plan was written. Some of the updates are minor such as revising dates in the text, while others address contentious issues such as automobile circulation. Whatever the update, staff attempted to keep the objective of the Task Force in mind.

Like the 1961 Core Area plan, the current Core Area Specific Plan strives to create a sense of place. By increasing commercial and residential opportunities in the downtown and creating a streetscape with unifying features such as benches and banners, the Core Area Specific Plan enhances the identity of the downtown. This plan gives the citizens of Davis the unique chance to act on opportunities identified over 30 years ago.

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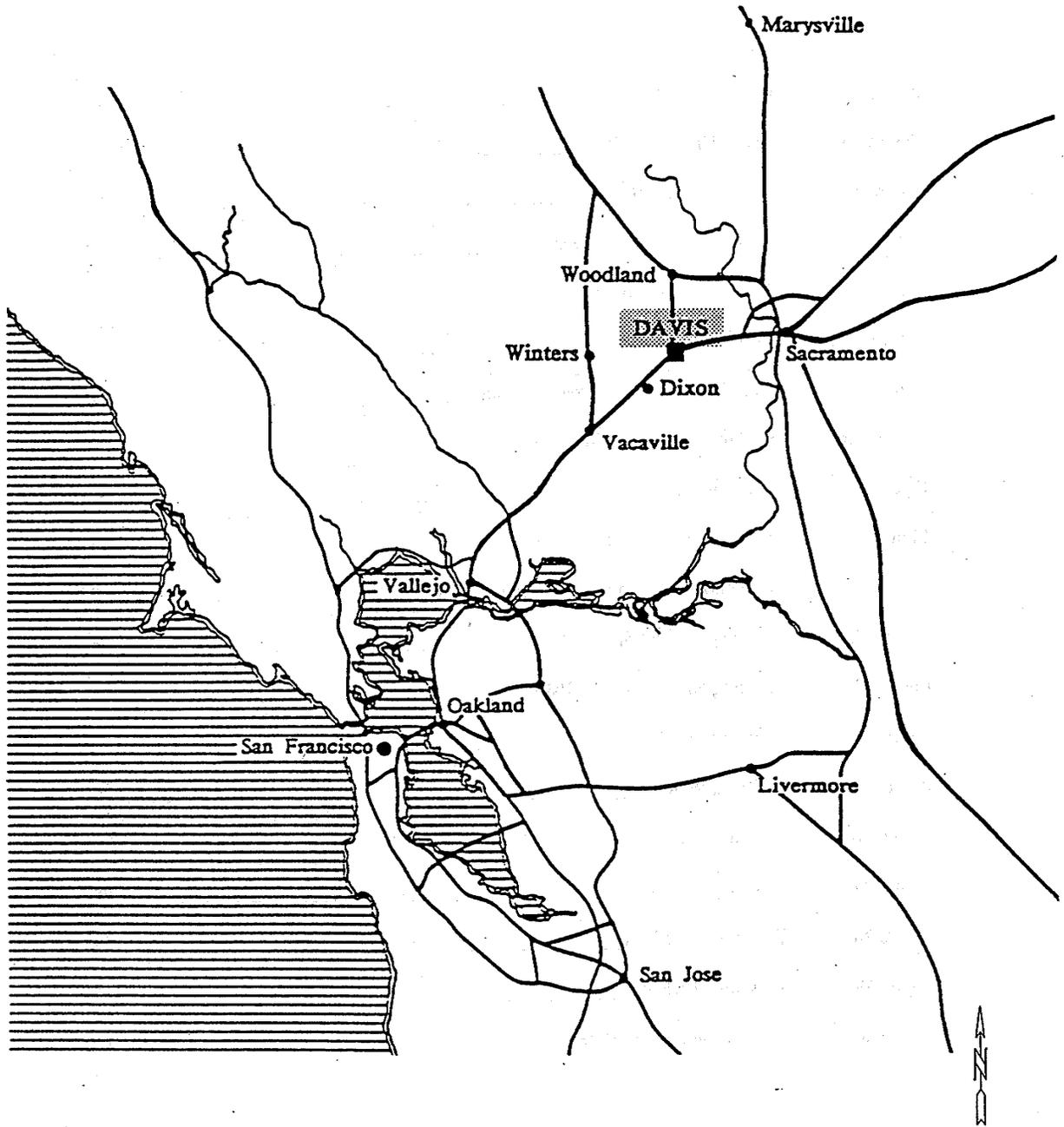
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FIGURE 2
REGIONAL LOCATION MAP



SECTION I. INTRODUCTION

1.1 PURPOSE

The purpose of the Core Area Specific Plan is to provide a comprehensive set of policies, guidelines and implementation strategies for promoting, guiding and regulating growth in the Core Area. Adopting and implementing the Core Area Specific Plan will allow the area to continue to function as the City's social, cultural, retail center, and professional and administrative office district in a manner that enhances pedestrian activity. The Core Area Specific Plan establishes the strategies which are required for the systematic execution of the City's General Plan for the area covered by the Core Area Specific Plan.

1.2 BACKGROUND

In April of 1989, the Core Area Specific Plan Task Force was formed at the direction of the City Council for the purpose of drafting a detailed plan to guide growth in the Core Area. The stated goal was to maintain the area as a strong and viable pedestrian oriented downtown center. The Core Area Specific Plan was prepared by the eleven member Task Force during 1989 and 1990. The Task Force had members representing landowner, business, design, and resident interests. The Task Force was charged with reviewing the General Plan and recommending changes in regards to the Core Area, and developing a detailed Specific Plan that would guide growth in the Core Area into the next century. To focus discussion, issues relevant to the Core Area were broken down into three categories: land use, circulation, and streetscape. In June of 1990, consensus was reached on the final issues and the first draft document was assembled during the remainder of that summer. However, some issues remained unresolved and the draft plan was not completed until 1994. The Core Area Specific Plan was amended in 2007 to reflect the intentions for development in portions of three special character areas as identified in the Downtown Davis and Traditional Residential Neighborhoods adopted in July of 2001 and amended in June of 2007.

The Core Area Specific Plan has been prepared in accordance with the California Government Code requirements for Specific Plans (Sections 65450-65457) and is intended for adoption by resolution by the City of Davis. As defined by the Code, the Core Area Specific Plan has been prepared for the systematic implementation of the City's General Plan for the Core Area. Fundamental to the Core Area Specific Plan are the policies and implementation strategies to "retain the Core Area as a multi-function downtown serving as the City's social/cultural center, primary retail business and professional and administrative office district in a manner that enhances pedestrian activity" as directed in the 1987 General Plan (General Plan Guiding Policy 2.2 C).

1.3 PROJECT LOCATION

The Core Area Specific Plan study area is located in the City of Davis, Yolo County, California (Figures 2 and 3). The study area encompasses approximately 152 acres which is bounded on the south by First Street, on the north by Fifth Street, on the west by A Street and on the east by the Southern Pacific Railroad tracks east of G Street except between Third and Fifth Streets where it is bounded by the alley west of I Street and between G and H Streets where it is bounded to the north by Eighth Street. The study area also includes the commercial area along G Street between Fifth Street and Eighth Street (Figure 4). The heart of the Core Area, referred to as the Downtown Core in the Core Area Specific Plan, is bounded by First and Third Streets and D Street and the railroad tracks. The Downtown Core contains the highest concentration of retail uses in the Core Area.

FIGURE 3
SITE LOCATION MAP

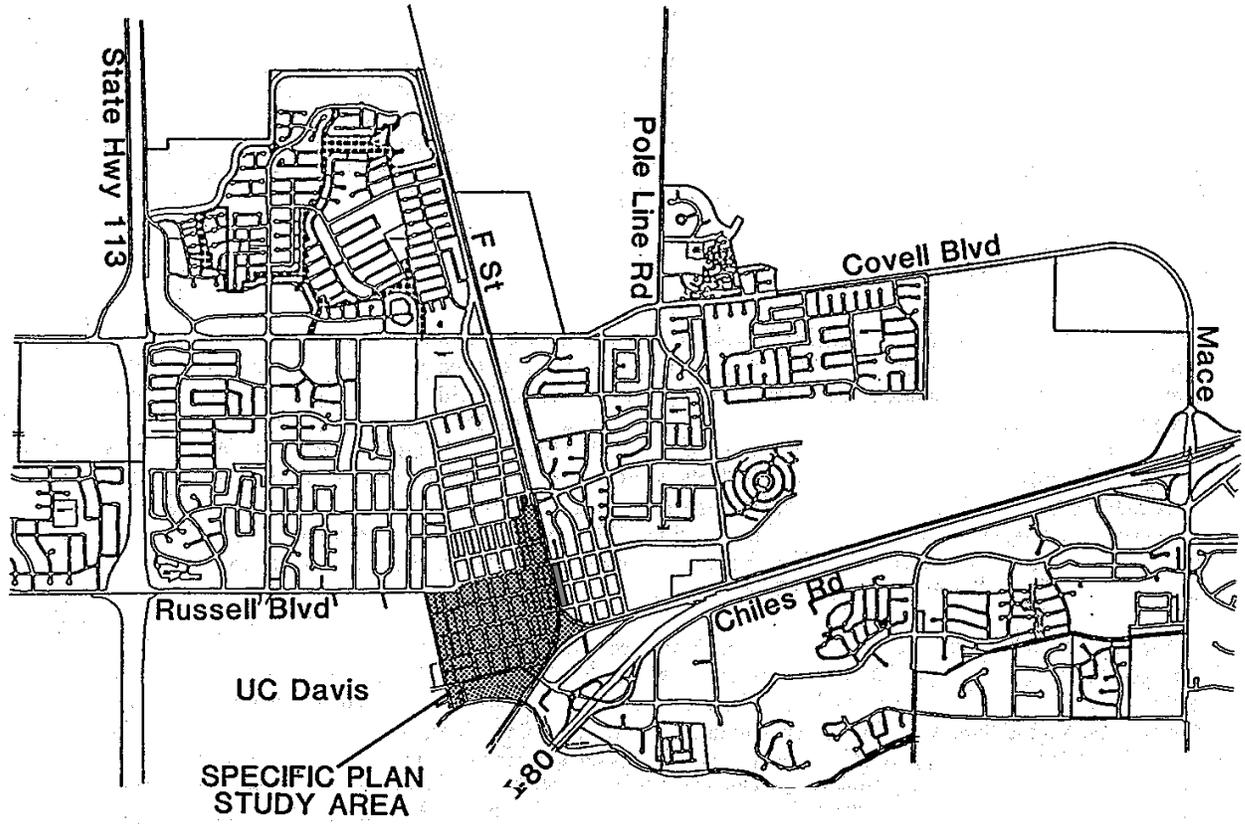
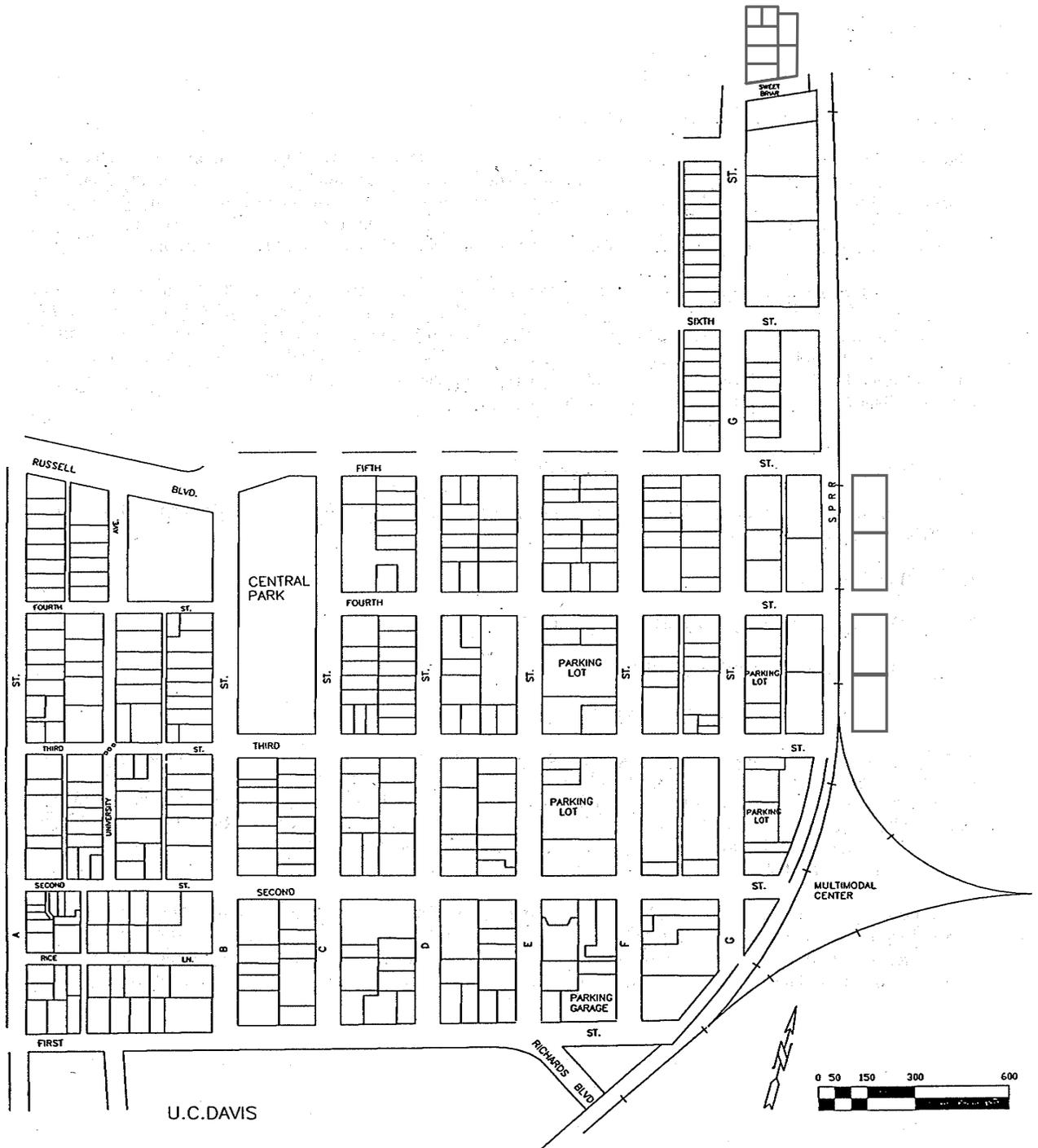


FIGURE 4
SPECIFIC PLAN STUDY AREA



1.4 RELATIONSHIP TO THE GENERAL PLAN

The Core Area Specific Plan has been prepared as a means of implementing the City's General Plan for the area covered by the Core Area Specific Plan. As stated in Government Code, Section 65454, the Specific Plan must be consistent with the City's General Plan before adoption. There are some differences between the General Plan and the Core Area Specific Plan regarding density, land use patterns, and the infrastructure necessary to serve it. Therefore, the process to adopt the proposed Core Area Specific Plan includes General Plan amendments.

The Guiding Policies in Section 2.5, Core Area, of the General Plan, are incorporated into the Core Area Specific Plan, Section II. The General Plan states, as a main guiding policy, that the Core Area shall be maintained "as the City's social/cultural center, including the primary center of retail business, and professional and administrative office district. Include features to enhance pedestrian activity" (General Plan Guiding Policy 2.5 A). The Core Area Specific Plan sets forth implementing strategies for carrying out this policy mandate. The Core Area Specific Plan establishes policies applicable to the Core study area only.

1.5 REGULATORY STATUS OF THE SPECIFIC PLAN

As required by law (Government Code, Section 65451), the Core Area Specific Plan includes text and diagrams specifying the following in detail:

1. Land use;
2. Distribution of major components of infrastructure;
3. Development standards and criteria;
4. A program of implementation measures.

The Core Area Specific Plan consists of Specific Plan maps, land use classifications, policies, explanatory material, and implementation strategies. Specific Plans are required to include text covering the distribution of infrastructure required to support the land uses described in the Specific Plan. The City's Major Projects Financing Plan details the public infrastructure projects necessary to implement the City's General Plan and, once the Major Projects Financing Plan is modified as called for in Section V of the Core Area Specific Plan, it serves as the infrastructure plan for the Core Area Specific Plan.

The Core Area Specific Plan will be adopted by resolution and will create a bridge between the City's General Plan and individual development proposals. Government Code, Section 65455, requires that any rezonings, tentative subdivision maps, and public works projects in the area covered by the Specific Plan be consistent with the Core Area Specific Plan. The Core Area Specific Plan supersedes any previous plans adopted for the Core Area.

CORE AREA SPECIFIC PLAN

1.6 INFRASTRUCTURE PLAN

The City's Major Projects Financing Plan, when modified as called for in the Core Area Specific Plan, details the public infrastructure projects necessary to implement the Core Area Specific Plan. The Major Projects Financing Plan, when modified, serves as the infrastructure plan for the Core Area Specific Plan and is incorporated by reference into the Core Area Specific Plan.

Section III, Circulation, of the Core Area Specific Plan, contains the development policies pertaining to the planned distribution, location, extent, and intensity of the major components of public transportation in the Core Area. For the Core Area Specific Plan proposals for streets, bikes, and transit routes, refer to Section III and to Figures 13 and 15.

For the current designation of streets in the Core Area, refer to Figure 22. See Figure 23 for the existing bicycle lanes and bicycle paths in the Core Area. Figure 14 depicts the existing and proposed signalized intersections, and Figure 19 shows the existing and future streetlights. See Figure 24 for existing bus routes. For the components of the existing water, sewer, drainage and gas lines in the Core Area refer to Figures 25 to 28.

1.7 SOLID WASTE

Davis Waste Removal, a private company, provides garbage pickup to the Core area. Davis Waste Removal services both residential and commercial customers. Collection services consist of one pick up per week. Commercial services vary depending on individual needs.

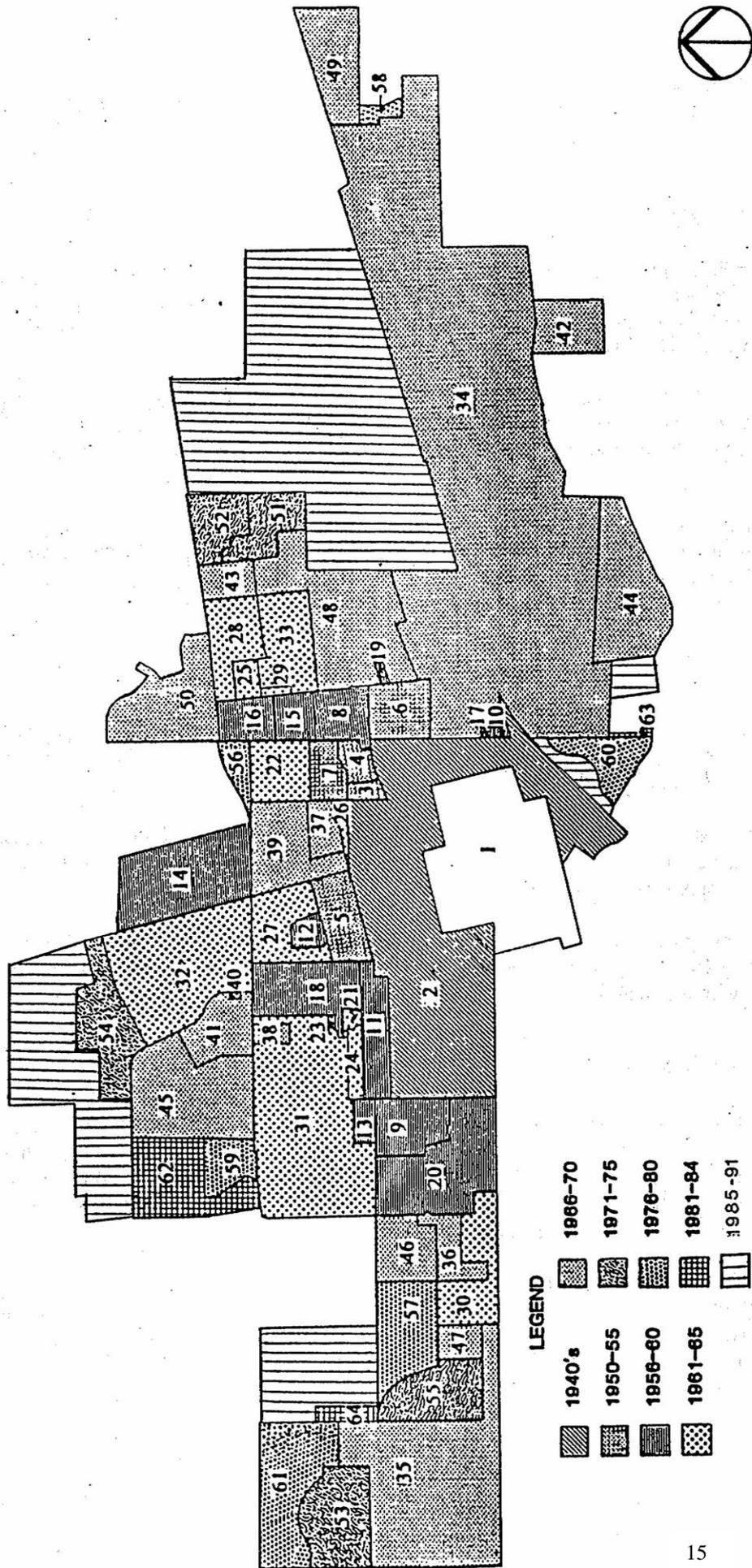
The disposal site for solid waste is the Yolo County landfill located at County Road 28H and Pole Line Road. By the end of 1995, approximately thirty-nine (39%) percent of the existing landfill capacity had been used; the remaining life expectancy is approximately 30 years.

Davis Waste Removal sponsors a voluntary recycling program by providing weekly curbside collection of newspaper, glass, cans and cardboard. Corrugated cardboard also is collected and recycled from commercial developments. Davis Waste Removal also provides for curbside collection of lawn and plant clippings.

1.8 USE OF THE CORE AREA SPECIFIC PLAN

The Core Area Specific Plan is intended to help designers and builders develop projects in the Core Area that are consistent with the community's desires and needs as outlined in the Core Area Specific Plan, and to guide staff members and decision makers in reviewing project proposals. When reviewing projects for conformance to the Core Area Specific Plan, each section of the Core Area Specific Plan shall be consulted to ensure that all of the Specific Plan requirements are met and incorporated into a project.

The three following sections, Land Use, Circulation and Streetscape, are devoted to the primary issue areas covered by the Task Force. A coordinated set of implementation measures, procedures, actions and techniques that will aid in carrying out the Core Area Specific Plan are discussed in these sections. Additional details on implementation, including a schedule and public improvement program, are contained in Section V.



LEGEND

	1940's		1966-70
	1950-55		1971-75
	1956-60		1976-80
	1961-65		1981-84
	1985-91		

1.9 SPECIFIC PLAN REVISION

The Core Area Specific Plan shall be reviewed annually to assure that it is still current and valid for guiding both short-term and long-term growth and development in the study area. If the Planning Commission finds that policy issues or changes in the basic assumptions or conditions have arisen and have made the Core Area Specific Plan deficient and outdated, they may recommend a reevaluation. Upon receiving a recommendation from the Planning Commission, the City Council may reconvene the Task Force for the purpose of discussions about possible revisions to the Core Area Specific Plan. To retain flexibility, the City may amend the Core Area Specific Plan without reconvening the Task Force.

1.10 SEVERABILITY CLAUSE

In the event that any regulation, condition, program, or portion of the Core Area Specific plan is held invalid or unconstitutional by a California or Federal Court of competent jurisdiction, such portions shall be deemed separate, distinct, and independent provisions, and the invalidity of such provisions shall not affect the validity of the remaining provisions thereof.

1.11 HISTORY OF DAVIS (excerpted from 1987 General Plan)

Davis was founded in 1868 as the Davisville rail depot, but the more significant date is 1906, when the University of California established the State Agricultural Experiment Station on 778 acres. In the 1920's, a Bachelor of Agriculture degree was offered by the University of California at Davis in conjunction with UC Berkeley. Following designation as a general campus in the 1950's, enrollment grew rapidly and professional schools were added during the next two decades. Although remaining outside the City's corporate limits, the University's development has driven population growth and has shaped the community's identity and values.

Between 1950 and 1987 the average annual growth rate was 6.4 percent per year as urban population grew from under 5,000 to 48,700. The relationship between UCD enrollment and total population has remained fairly steady for the last 20 years, with the ratio of city population to UCD enrollment generally around 2:1. In 1986, 94 percent of the 19,300 UCD students and half the 8,500 full-time faculty and staff lived on campus or in the city. Figure 5 shows how the city has grown since incorporation in 1917.

The 9,900 non-UCD jobs in Davis are mainly in services and retailing. In the City of Davis median age in 1990 was 25.5 (compared with 28.9 for Yolo County) and 8.4 percent of residents were over 60 (compared with 12.8 percent for Yolo County). Employment and age data source: 1990 Census.

Davis is known for bicycles, energy conservation, and a preference for slow growth as expressed by passage of Measure L in June 1986. Davis' notable physical characteristics are its small scale in relation to UCD, innovative neighborhood design, and a traditional downtown.

1.12 EXISTING LAND USES AND ZONING

Existing Land Uses

In April 1993, the City Planning Department conducted a land use survey of the entire Davis planning area in preparation for the 1994 General Plan Update process. Staff surveyed all existing land uses, identifying the existing land uses utilizing the 1987 General Plan land use categories. The information obtained in the field was compiled by the General Plan planning sub-areas. The Core Area is a General Plan planning sub-area.

For the Core Area, the land use categories are: retail, office, single family residential, duplex (residential), apartments and parks. Retail uses include commercial enterprises such as stores, restaurants, hotels, and service commercial uses. Office uses include administrative, professional, government and medical offices. For the retail and office categories, data was compiled in square feet. Single family uses include single family units and row houses with four or fewer units. Apartment uses include apartments and development types with five or more units in a structure. For the single family, duplex family and apartment categories, the data was compiled in dwelling units. The data for the parks category was compiled in acres.

The data was compiled, for the Core Area, by traffic analysis zones (TAZ). These zones group several blocks together for traffic model purposes. There are twelve of these zones in the Core. (see Table 1. Existing Land Uses in the Core Area for tabulation by TAZ; also refer to Figure 6. Existing Land Uses for map of existing uses by TAZ).

In 1993 in the Core Area, there were approximately 475,996 square feet of retail uses, 306,774 square feet of office uses, 506 residential units, and five acres of park in Central Park. All of the zones, except three, have a mixture of retail, office and residential uses. TAZ 133, 134 and 139 (the Downtown Core) consist entirely of retail and office uses. The blocks become more residential in nature going west towards A Street and north of Fifth Street. Refer to Figure 6 for a depiction of the land uses in the Core Area. The circled numbers correspond to the TAZ numbers in Table 1.

Existing Zoning

There are thirteen zoning districts in the Core Area Specific Plan study area (Figure 7). One of these, the Core Area Design Combining District, is an overlay district which applies to areas zoned Central Commercial, Mixed-Use and Core Area Residential Infill. The purposes of the Core Area Design (C-D) Combining District are to: provide for the harmonious blending of residential and commercial uses; preserve the older architectural styles and encourage the intermingling of compatible structures; enhance the pedestrian and bicycle-oriented character of the core; and preserve the tree-shaded ambiance.

The Downtown Core area lies within the Central Commercial (C-C) District. The purposes of this district are to: implement the core plan; provide for an increased variety and density of commercial activities; to preserve older architectural styles where feasible; to encourage a harmonious intermingling of other structures; to permit residential uses where feasible; to promote pedestrian use and enjoyment of the core; and to provide an area of intense commercial activity.

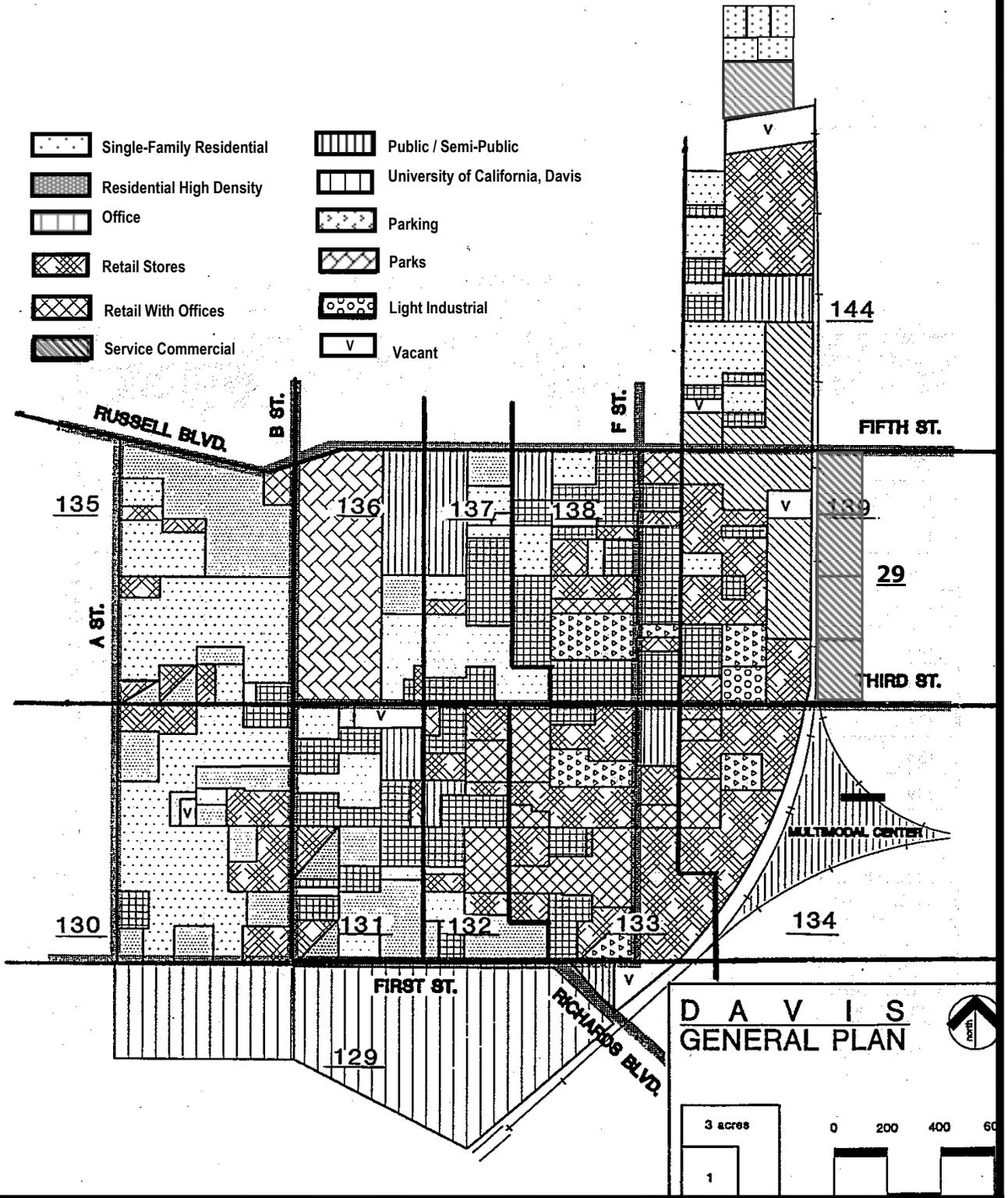
CORE AREA SPECIFIC PLAN

TABLE 1. EXISTING LAND USES IN THE CORE AREA (a)

TAZ (b) NUMBER	RETAIL SQ. FT. (c)	OFFICE SQ.FT.	SINGLE FAMILY	DUPLEX UNITS	APT. UNITS	PARK ACRES
29	17,955	6,325	0	0	0	0
129	0	0	0	0	0	0
130	42,053	5,358	32	14	112	0
131	11,861	38,858	16	0	71	0
132	29,900	47,167	2	0	28	0
133	224,705	81,799	0	0	0	0
134	53,439	8,656	0	0	0	0
135	11,684	5,732	32	4	106	0
136	0	0	5	0	12	5
137	2,625	10,974	11	8	2	0
138	18,610	69,284	4	8	14	0
139	46,555	19,588	1	0	0	0
144	34,564	23,335	13	10	11	0
TOTAL	493,951	317,076	116	44	356	5

a. 1993 land use survey conducted by the Davis Planning Division.
b. Refer to Figure 6: Existing Land Uses for the corresponding TAZ.
c. Sq. ft. is square feet.

FIGURE 6
EXISTING LAND USES



CORE AREA SPECIFIC PLAN

The Core Area Residential (C-RI) District has the following purposes: to implement the core area plan; to preserve and protect the residential character of the district, its tree-shaded ambiance and its older architectural styles; to promote a greater variety of residential uses; and to insure that new structures and uses harmonize with the surrounding residences.

The Mixed Use (M-U) District calls for: implementing the policies of the core plan; preserving the older architectural styles and encouraging an increased variety and intermixture of residential and commercial activities; and enhancing the tree-shaded ambiance, pedestrian usage and character of the district.

The purposes of Planned Development District No. 2-86B, (the area bounded by A and B Streets and First and Fifth Streets) are to: stabilize and protect the residential character of the area while allowing limited commercial uses combined with higher residential densities along Third Street and portions of B Street; retain and renovate existing homes wherever possible outside of the B and 3rd Streets Visioning project boundary; and retain Landmark Trees, Trees Worth Saving, and all other significant trees whenever possible.

Amendments to the zoning designations and development regulations for portions of PD 2-86A located along 3rd Street and on the west side of B Street between 2nd and 4th Streets have been made to allow transition of these areas to more urban building forms and higher densities.

1.13 RECENT CONCERNS AND ISSUES

The Core Area has been fostered over the years by policies limiting shopping centers on the outskirts of Davis to a size suitable for neighborhood support services, and encouraging community-wide retail services to be located in the Core Area. This support includes General Plan policies to this effect. The Core Area Specific Plan is intended to further strengthen downtown Davis as the community's social, cultural, recreational, and retail center in a manner that enhances pedestrian activity. A number of issues related to the Core Area have surfaced over the past few years, including those discussed below.

Land Use: Attention has been focused on the trend towards conversion of ground floor level space to non-retail uses and on attracting a large-scale retail anchor to the Core Area. Specific attempts have been made to provide additional retail at the Aggie Village and Fifth and G Streets sites.

The loss of retail uses in the Core Area is a concern both because retail uses attract people downtown, and because the uses generate important sales tax revenues for the City. In response to this concern, the City Council has approved, in concept, a restriction prohibiting non-retail conversions in the area bounded by First and Third Streets and E and G Streets.

The stabilization of existing residential neighborhoods and the development of new residential units in the Core Area is also seen as critical in maintaining a healthy and viable downtown. The retention of a residential base in the Core Area is seen as crucial to the success of the pedestrian environment of the downtown.

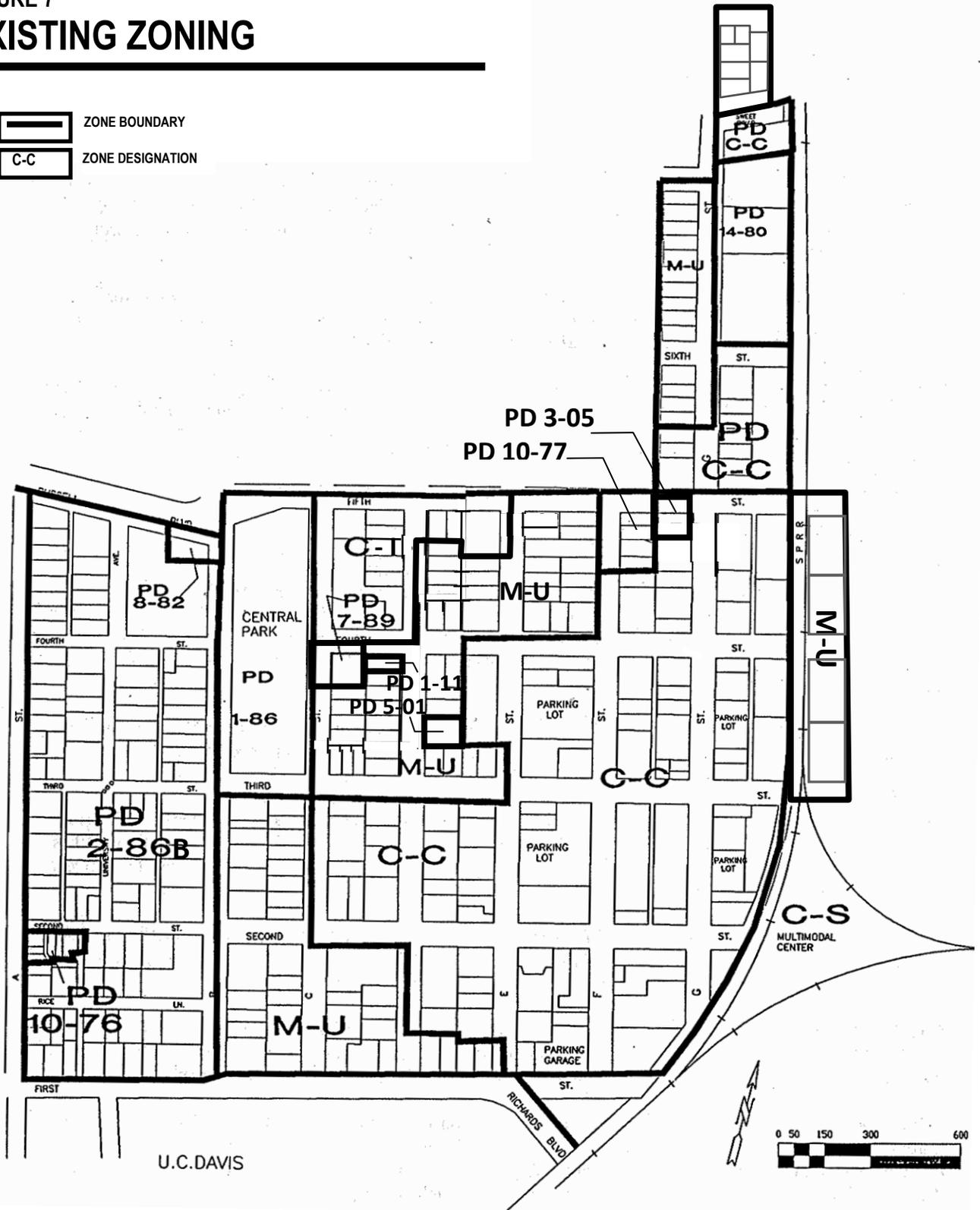
Circulation: Automobile circulation in the Core Area reflects the limited automobile access to downtown. Paradoxically, the features that create this situation are those which have contributed to the economy of Davis, and the excellent pedestrian environment in the Core Area is in part due to the limited automobile circulation.

The Southern Pacific Railroad tracks border the Core Area on the south and along most of the east side, and the University of California at Davis Campus restricts circulation to the west. Of the seven east-west streets in the Core

CORE AREA SPECIFIC PLAN

Area (First through Seventh), only three (Third, Fourth, and Fifth) are through streets to the east, and only Fifth continues to the west. Eight north-south streets serve the Core Area (A through G Streets, plus University Avenue). Of these,

FIGURE 7
EXISTING ZONING



CORE AREA SPECIFIC PLAN

only four (A, B, D, and F) extend past Eighth Street to the north, and only Richards Boulevard leaves the Core Area to the south.

This street pattern, compounded by the fact that Richards Boulevard is one of only three access points to South Davis, has always limited options when traveling to or through the Core Area by automobile. Growth in recent years in North and South Davis has caused traffic from one part of Davis to other parts of Davis to be routed through the Core Area, even if not bound there.

The City Council approved the design of the Richards Boulevard underpass in the summer of 1996. The project calls for four auto lanes with separated bike paths and pedestrian facilities. Impacts of the project are discussed in the Richards Boulevard EIR, released in January 1996.

Parking: Parking for employees and customers of downtown businesses has been an ongoing concern since Davis' population began to grow rapidly in the 1960's. In the years since, a variety of measures have been implemented to deal with the problem. These began with the creation of Special Assessment Districts to construct and maintain public parking lots along E, F, and G Streets. Core Area businesses within the districts are assessed based on their proximity to the parking.

In the early 1980's, the City experimented with pairs of one-way streets which allowed on-street parking to be restriped for angle parking. This increased the supply of parking spaces, but as a program it was discontinued because of the circulation difficulties caused by one-way streets and conflicts with cars backing out into traffic. However, recent requests from businesses for the program has resulted in the limited reinstatement of angled parking in the Core Area.

Recent parking programs include restrictions on long-term (over two hour) parking in the Core Area coupled with a permit program for area residents. This has caused spillover employee parking to occur in the residential neighborhoods just east and north of the Core Area. Still, Chamber of Commerce studies of parking in the two-hour restriction area concluded that over half of the parking spaces are consumed by employees who rotate their cars from space to space throughout the day. This information has prompted a voluntary program by the downtown merchants to encourage employees to park outside of the Core Area or to use alternate transportation.

The most recent actions affecting parking in the Core Area were the Redevelopment Agency's construction in 1989 of a 200 space parking structure at First and F Streets and the surfacing of the boy scout cabin site at 616 First Street in 1995 which provided 52 parking spaces. Other similar projects are planned at this time, but with no set construction date.

Amenities: A strong attempt to upgrade the appearance of the Core Area has been made recently by the City, the Chamber of Commerce, and the Downtown Business Association. This has resulted in a program to improve the appearance and quality of lighting, landscaping, and overhead banners announcing major downtown events. In addition, a multi-disciplinary City work-team has begun the process of selecting styles for street furniture and sites for amenity upgrades. Their work overlaps the Streetscape section of the Core Area plan and could be used as demonstration projects to show how the urban design improvements called for in this plan enhance the downtown.

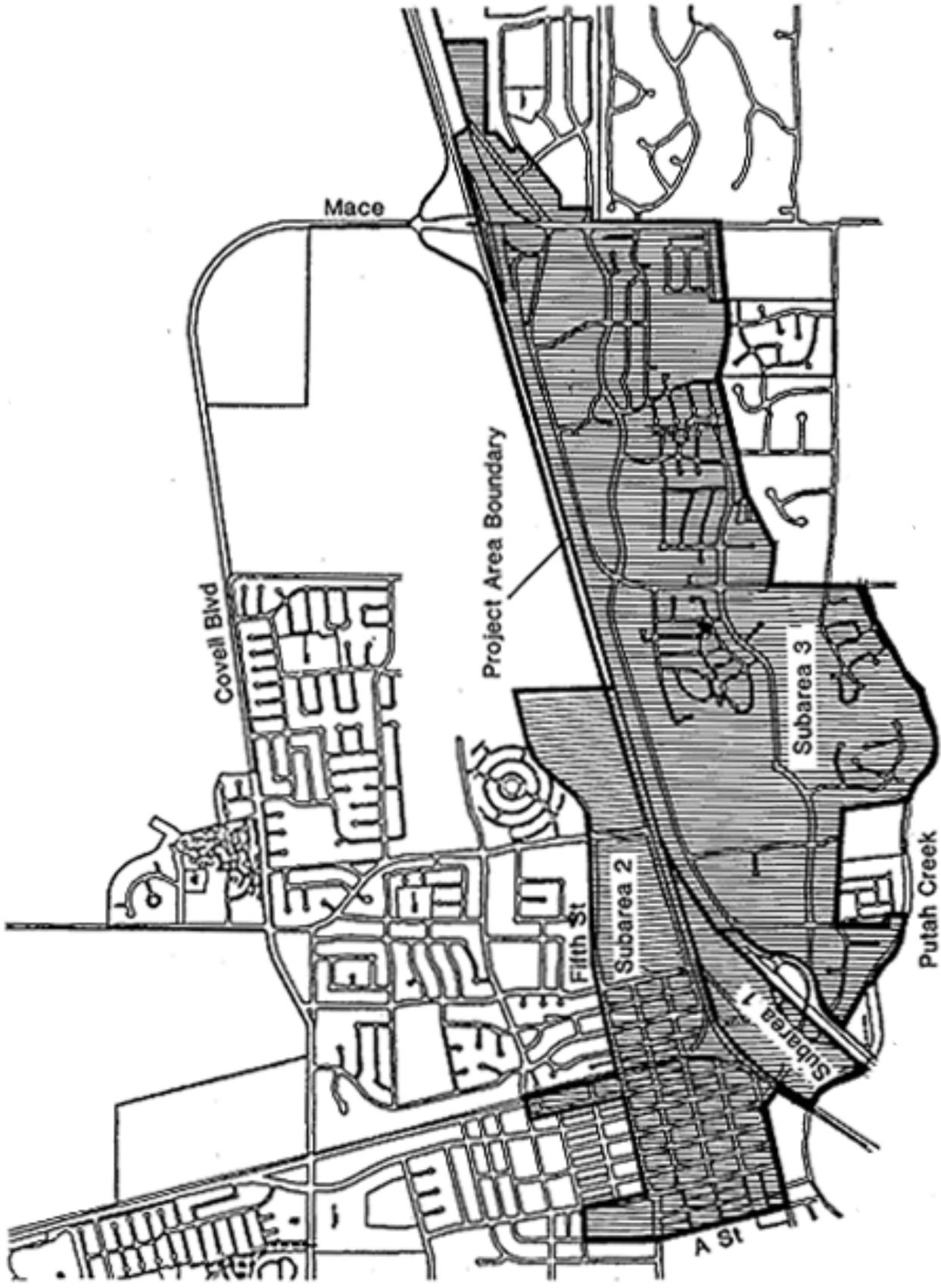


FIGURE 8 REDEVELOPMENT AGENCY PROJECT AREA

SECTION II. LAND USE

2.1 INTRODUCTION

The general goal of the land use policies for the Core Area is to retain and foster those types of uses that allow the Core Area to make a major contribution to the social, cultural and economic life of the City of Davis. In order to achieve this general goal, the land use policies in the Core Area Specific Plan favor a mix of uses. Currently, office and service functions, such as banks, real estate firms, and certain types of retailing (for example, restaurants), are well represented. The stabilization of existing, and the development of new, general retailing and housing require special attention. The retention of a residential population base in the Core Area is crucial to the success of the pedestrian environment of the downtown.

Not only the uses, but their pattern and scale, are important in maintaining the character of the Core Area. Intensification of uses and major new developments are directed to particular locations in order to retain the Core Area's pleasant small city atmosphere. Development in the Core Area shall adhere to the land uses designated as appropriate for certain areas as set forth herein and on the Core Area Specific Plan Land Use Map (Figure 9).

2.2 AFFORDABLE HOUSING

The Core Area lies within the City of Davis' Redevelopment Agency Project Area. The Project Area consists of approximately 1,450 acres in South Davis, Central Davis and Olive Drive (Figure 8). The Core Area is in Subarea 2 of the Project Area. California State law has certain requirements for affordable housing within the Project Area. If housing is built or rehabilitated by the Redevelopment Agency, thirty percent of the units must be affordable to moderate income households. As of January 1996, the Redevelopment Agency has not constructed, nor caused to be constructed, any residential units within or outside the Redevelopment Project Area.

There are requirements imposed on private development within the Redevelopment Agency Project Area. Fifteen percent of all units constructed or rehabilitated within the Project Area must be affordable to low or moderate income households. Six percent of the total units must be affordable to very low income households. These units must remain affordable for the term of the Redevelopment Plan, or through the year 2022. These required amounts are aggregate totals for the Project Area and are not project specific. Both the very low income requirement and the low/moderate income requirement are expected to be met or exceeded in accordance with state law.

There are also affordable housing requirements separate from the Redevelopment Agency requirements. These have been established through the City of Davis' inclusionary policies and ordinance establishing an affordable housing program for the City. Any residential development project with five or more units needs an affordable housing component. The "Affordable Housing Ordinance" requires that from twenty-five percent to thirty-five percent of all new housing must be affordable to certain income levels. Small developments may pay fees in lieu of providing affordable housing on site. These requirements are project specific. As of January 1996, no units have been built in the Core Area or in lieu fees paid pursuant to the "Affordable Housing Ordinance" requirements. There is currently one request for Agency assistance in the provision of four affordable units in the Core Area.

2.3 GENERAL PLAN GUIDING POLICIES

Following are the Guiding Policies from the City's current General Plan, Section 2.5, Core Area. These are incorporated into the Core Area Specific Plan as overall guiding policies. The Core Area Goals from the pending General Plan update appear in italics and are included to show continuing consistency of the core plan.

Guiding Policy 2.5 A

Maintain the Core Area as the City's social/cultural center, including the primary center of retail business, and professional and administrative office district. Include features to enhance pedestrian activity.

Guiding Policy 2.5 B

Add strong retailers to increase shopping convenience, employment and sales tax revenue.

Guiding Policy 2.5 C

Keep non-medical professional and business offices in the Core Area.

Guiding Policy 2.5 D

Accommodate new buildings with floor area up to three times site area, but maintain scale transition and keep enough old buildings to retain small-city character.

Guiding Policy 2.5 E

Plan off-site parking to meet office and retail demand.

Guiding Policy 2.5 F

Strengthen the visual connection between the Core and the University.

Guiding Policy 2.5 G

Add apartments in the Core.

-[Growth Management Goal 1] Maintain and enhance the Core Area as a vibrant, healthy pedestrian-oriented downtown that serves as the city's social/cultural center and primary retail and business center.

-[Growth Management Goal 3] Develop the Core Area as a livable mixed-use downtown with office, residential, retail and cultural/recreational facilities within convenient walking distance.

-[Economic Development Goal 1] Maintain and enhance the Core Area as a vibrant, healthy pedestrian-oriented downtown that serves as the city's social/cultural center and primary, but not exclusive, retail and business district, in addition to developing the core into a livable mixed use neighborhood.

-[Urban Design Goal E] Maintain and enhance the Core Area as the City's social/cultural center and primary retail and business district.

The General Plan update process is scheduled to be completed by early 1997. The Core Area goals from the General Plan update listed above and the corresponding implementation strategies serve to support the Core Area Specific Plan. If the updated Core Area goals, policies, and actions are adopted without substantial change, the Core Area Specific Plan will remain consistent with the updated General Plan.

2.4 SPECIFIC PLAN LAND USE MAP

The Core Area Specific Plan Land Use Map (Figure 9) depicts the areas designated for the different land use classifications described below. A development application may be found to be consistent with the Core Area Specific Plan if it adheres to the land uses designated as appropriate for the location of the project and if it is determined that the project meets the policies and standards set forth in all sections of the Core Area Specific Plan.

The Core Area Specific Plan Land Use Map designates the area bounded by First and Third Streets and D Street and the railroad tracks as Retail Stores (Downtown Core). This is the most intensive retail area. To the north and west are sites designated Retail with Offices which calls for more mixed use. Single-family and multifamily are the predominant land use classifications west of B Street. Retail with Offices is shown on several properties fronting the west side of B Street, between Third Street and Fourth Street east of C Street, between First Street and Third Street east of B Street west of D Street, and the properties along both sides of Third Street between the campus and B Street. Retail with Offices is also shown between Third and Fifth Streets east of the Southern Pacific Railroad tracks and the alley west of I Street, and between Sweetbriar Drive and Eighth Street east of G Street and west of H Street. There are areas designated Office south of Fifth Street around D and E Streets and north of Fifth along G Street. North of Fifth Street between G Street and the railroad tracks is an area designated Service Commercial. The other land use classifications are shown on various sites throughout the Core Area Specific Plan study area.

2.5 LAND USE CLASSIFICATIONS

The land use classifications described below cover both public and private development and serve as a guide for zoning regulations and subdivision designs. These classifications apply to the uses designated on the Core Area Specific Plan Land Use Map (Figure 9).

Residential

Some public and semipublic uses, such as schools, child care, group care, and religious assembly, may be appropriate within areas designated for residential use. Greenbelts and other private recreation areas are also included within residential areas.

Low Density: Attached and detached single-family units, including mobile homes; all lot sizes, duplexes, triplexes and row houses of four or fewer units. Locations designated for single-family development can be uniform or can include a mix of attached and detached units, provided average density in new development areas does not exceed 2.5 to 4.2 units per gross acre (about 3.1 to 5.6 units per net acre). Lower densities may be permitted.

Infill projects may have a gross density exceeding the 4.2 units per acre limit. Infill projects, in this case, are those projects which were in the City limits prior to the adoption of the General Plan (December 24, 1987) and also meet one of the following criteria: had an approved housing allocation; or had an approved intent to allocate and, if necessary, had a rezoning application on file in the Community Development Department; or had an approved City subdivision or greenbelt agreement.

Medium Density: Single-family or multi-family residential with densities from 4.2 to 10.0 units per gross acre.

First Street Transitional District: Includes professional offices, single-family residential and combined residential/office uses. In this district, the residential scale and character of the University Avenue neighborhood

CORE AREA SPECIFIC PLAN

shall be retained. It is a goal for this district that parking impacts be minimized. In the area designated as University Avenue Transitional District along B Street, there shall be no on-site parking allowed on the B Street property frontage nor will on-site parking access from, or curb cuts on, B Street be allowed. Required on-site parking shall be provided in the rear with access from the alley. The rear on-site parking area shall be screened.

B Street Transitional District: Intent to create mixed use urban village including higher density, compact/attached ownership residential units, live/work, and professional offices/services oriented to creative and other occupations generating low traffic generation and lower parking demand. Single family, two-family, condominium dwellings and duplexes may also be included. No new on-site parking will be allowed in front yards along B Street. Required parking shall be provided in the rear with access from the alley and shall be screened.

University Avenue Residential Overlay District: Includes single-family detached, single-family attached, duplexes, condominiums and townhouses. Densities are limited to 12 units per gross acre (approximately 9 units per net acre). The site plan and building design of any new or intensified residential use shall be compatible and complimentary with the historic cottage and single-family character of the University Avenue neighborhood. Living groups may be considered in this area, but only through a conditional use permit process which shall include a noticed public hearing.

Multifamily: Includes apartment, condominium, town house, row house and other development types with five or more units in a structure. Densities are limited to 10 to 15 units per gross acre, except in that portion of the Core Area east of B Street where the maximum density is 30 units per net acre.

For multifamily uses (more than three units) in the area along 3rd Street between A Street and B Street, up to 30 units per net acre are allowed.

For multifamily uses (more than three units) on the west side of B Street between 2nd and 4th Streets, densities ranging from up to 22-24 units per net acre are allowed for town or row homes. Densities up to 45 units per acre may be allowed on the properties located at 225 and 229 B Street (Assessor's Parcel Numbers: 070-065-11 and 070-065-12), subject to site-specific review and approval, for ownership projects that provide underground parking, 1-2 bedroom units, or open space significantly exceeding zoning requirements.

The single-family and multifamily densities listed do not include density bonuses for inclusion of housing for low- and moderate-income households or seniors as required by Government Code, Section 65915. See General Plan Housing Element policies 5.1 N and 5.2 F. Small-scale support facilities, such as limited medical or retail facilities, may be determined to be appropriate uses within housing complexes designed for senior and/or disabled residents.

Nonresidential

Core Retail Stores (Downtown Core): The Downtown of the Core Area (the area bounded by First and Third Streets and D Street and the railroad tracks) is intended to provide a concentration of stores and uses that allows each to benefit from the presence of the others. Retail uses at ground floor level with professional and administrative offices and residential units encouraged for upper stories in this zone within the Core Area. Cultural and entertainment uses are also permitted at ground floor level. Total floor area may reach three times the site area. Parking structures are excluded from the calculations of floor area ratio.

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As there is a significant need for child-care facilities to be included in private developments, child-care facilities may be appropriate uses within any of the following land-use classifications.

Core Retail with Offices: Mixed retail and office uses with retail uses dominant at ground floor level and offices encouraged as tenants for upper stories. Uses need not be mixed on individual parcels. Retail uses include stores, restaurants, cultural, entertainment, hotels and commercial recreation (such as recreation centers and athletic clubs). Offices include business, professional, government and medical offices. Apartments and owner occupied condominiums and town homes may be included and are encouraged as tenants for upper stories. Single-family, two-family and duplexes may also be included.

Total floor area in the Retail with Offices District located along 3rd Street between University Avenue and B Streets and on the northwest corner of B and 2nd Streets are allowed a floor area ratio (FAR) of up to 2:1 maximum including bonus: commercial only 1:1, mixed use 1:1.5; 0.5 FAR bonus allowed for preservation of designated historic structure, underground parking or “Trees Worth Saving”; 0.2:1 FAR bonus for plaza or preservation of “Trees of Significance.” Parking structures are excluded from the calculations of floor area ratio.

Transitional Boundary: This boundary recognizes the need for a transition between higher intensive commercial and office land uses and lower intensive uses. A mixture of commercial, office, and residential uses are encouraged. It is appropriate to consider lesser intensive, yet mixed use zoning in this area.

Core Service Commercial: Automotive sales and service, lumber yards, nurseries, storage, equipment rental, repair services and wholesale businesses. Convenience retail stores, restaurants and limited professional/administrative or veterinary offices are included. Maximum floor area ratio is 40 percent.

Public and Semipublic: Public, quasi-public buildings, libraries, city halls, police and fire departments, schools, child-care facilities, religious institutions and utilities. Some semipublic uses may be located in residential areas.

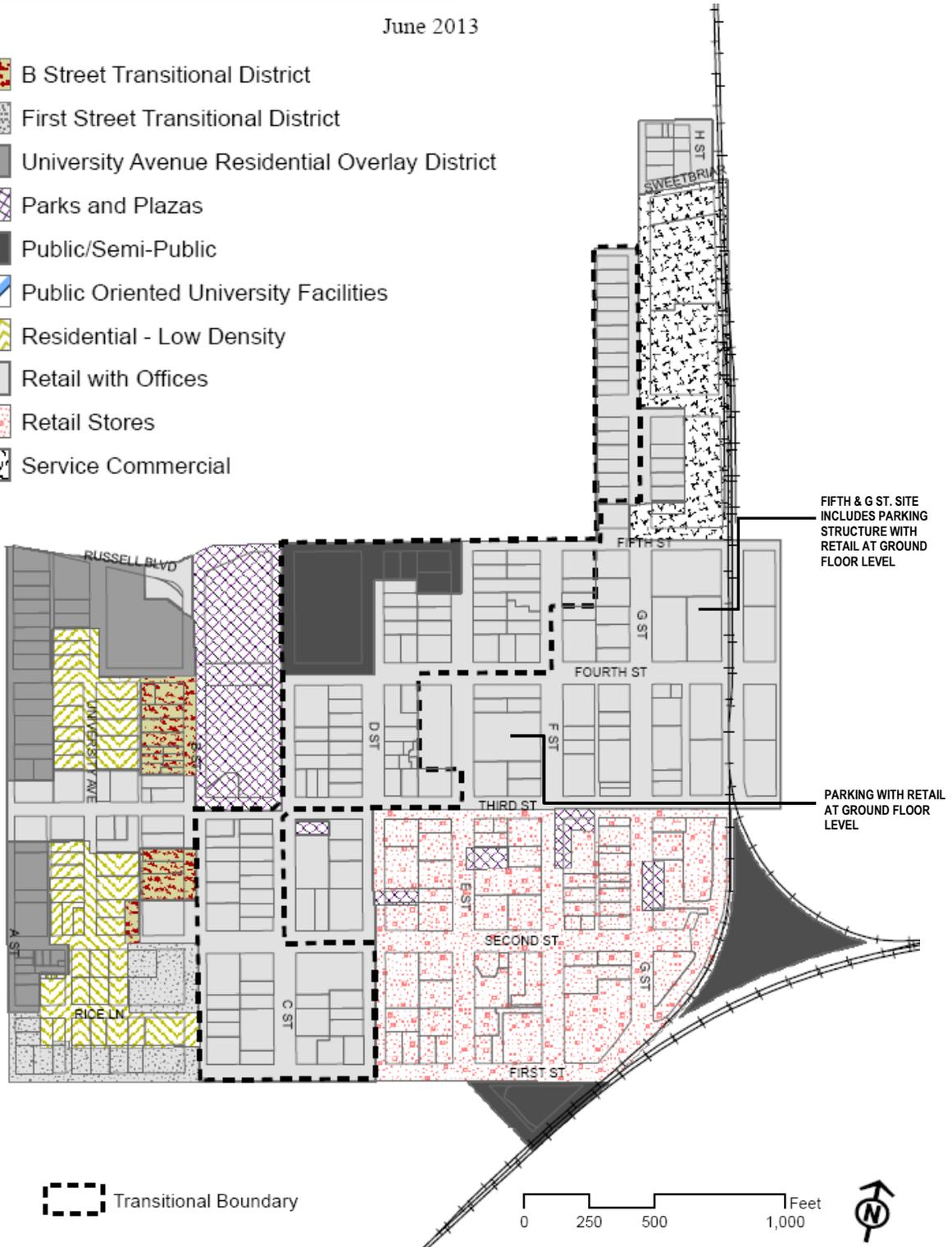
Public-oriented University Facilities: University of California, Davis campus.

Parks and Plazas: Existing and proposed public and private parks, plazas, recreation sites and public facilities.

FIGURE 9 LAND USE MAP

June 2013

-  B Street Transitional District
-  First Street Transitional District
-  University Avenue Residential Overlay District
-  Parks and Plazas
-  Public/Semi-Public
-  Public Oriented University Facilities
-  Residential - Low Density
-  Retail with Offices
-  Retail Stores
-  Service Commercial



2.6 LAND USE POLICIES

1. **Policy:** A mix of uses -retail stores, restaurants, cultural centers, entertainment, services, upstairs offices and dwelling units- is now and shall remain characteristic of the Core Area.

Explanation: This mix promotes street life, sales tax revenues, safety, shared parking and walking between multiple destinations within the Core Area. There needs to be a careful mixing of uses to provide for the needs of the community without violating the privacy and security of the residents of the Core Area.

Implementation: A. Mixed uses in the Core Area shall be encouraged.

B. The City shall take measures that encourage new businesses to locate in the Core Area.

C. Pedestrian and customer intensive retail businesses shall be encouraged to locate on ground floor level in the Downtown Core (Retail Stores) area.

D. The City shall promote development that brings maximum economic life and stability to the Core Area and which enhances the pedestrian and architectural character of the downtown. The City shall implement the specific economic development program, goals and strategies for the Core Area.

E. The City shall pursue funding sources for revitalization programs in the Core Area. These sources might include Community Development Block Grants (CDBG), Business Improvement Districts (BID) or some type of City sponsored grants/matching funds.

F. Provide for the continuation of an open air farmers' market and other special outdoor events that are compatible with existing Core Area uses.

G. Protect existing residential neighborhoods and their neighborhood character.

H. Where feasible, encourage the adaptive reuse, renovation and/or rehabilitation of existing residential facilities.

CORE AREA SPECIFIC PLAN

I. The development of dwelling units, including senior housing, shall be encouraged in the Core Area. A variety of mechanisms to promote housing in the Core Area shall be developed by staff. This includes, but is not limited to, the promotion and development of housing in upper stories in the Downtown Core (Retail Stores) area with the possibility of reduced off-street parking requirements and reserving parking spaces in the First and F Streets parking structure for the tenants (Figure 10).

J. The City shall explore the use of Community Development Block Grants (CDBG) and/or Redevelopment housing funds for the development of new, or the rehabilitation of existing, residential units, or to support organizations who will develop or rehabilitate residential units in the Core Area. The Grants Coordinator may apply for additional funds as available.

- 2. Policy:** Cultural and entertainment uses are to be encouraged in the Core Area.

Explanation: Cultural and entertainment uses are likely to be pedestrian and customer intensive, lively, and of visual interest. These types of uses and the many restaurants in the downtown can benefit from the presence of each other.

Implementation: A. Promote new cultural activities, entertainment, live performance theaters, clubs, and art galleries in the Core Area. These types of uses are appropriate at ground floor level.

B. Complete the renovation of the old Varsity Theater into a community theater.

- 3. Policy:** While non-retail uses are not prohibited, pedestrian and customer intensive retail uses are preferred at ground floor level in the Downtown Core (Retail Stores) area. If it becomes necessary, incentives and disincentives shall be used to promote retail uses at ground floor level. The first floor of buildings in the Downtown Core need to be pedestrian oriented (Figure 11).

Explanation: Retail uses are key to the prosperity and liveliness of downtown. They are especially important on the ground floor because they help create an interesting, enticing pedestrian environment. A survey done in 1990 of existing ground floor uses in the Downtown Core (First Street to Third Street and D Street to the railroad tracks) by the Task Force revealed that 51 percent of the ground floor space was devoted to sales tax generating retailing and another 20 percent to restaurants. One-quarter of the uses were classified as services and four percent were vacant or residential property. Task Force members believe that this is an adequate share of ground floor space given over to retail and restaurant use and that the pattern of use is not significantly different from that of a decade ago. Thus, the Task Force judged that it was not necessary to require ground floor retailing by ordinance. If there is a downward trend in retail uses in the Downtown Core, then incentives and disincentives will be necessary.

Requiring the first floor of buildings to be built up to the sidewalk in the Downtown Core area facilitates window shopping, browsing, people watching and social interaction and helps create a lively pedestrian atmosphere. Having a concentration of uses within short walking distances of each other also encourages pedestrian activity and interest.

Implementation: A. Annual monitoring by Planning staff of the share of ground floor space occupied by different categories of uses.

CORE AREA SPECIFIC PLAN

B. If it is determined to be necessary, because of a downward trend in retail uses or because of a significant loss of retail uses at ground floor level (that is, less than 50 percent of ground floor level devoted to retail uses) in the Downtown Core (Retail Stores) area, the development of regulations and incentives to require retail only at ground floor level in the Downtown Core area shall become mandatory. These regulations and incentives shall include, but are not limited to, amending the Core Area Specific Plan and the Zoning Ordinance to require retail only at ground floor level in the Downtown Core (with possible involvement of the Redevelopment Agency).

C. Amend the Zoning Ordinance to include a definition of "commercial" in relation to the Core Area zonings and the Core Area Design (C-D) Combining District. The definition shall reflect the intent of "retail" use at ground floor level where the word "commercial" is used.

D. Require that the first floor of buildings in the Downtown Core (Retail Stores) area be pedestrian oriented.

- 4. Policy:** The first floor of Core Area buildings shall either be at the same elevation as the sidewalk or designed such that pedestrians are encouraged to enter the building. There also needs to be a concentration of uses with a clear identity and with short walking distances between the uses.

Explanation: Buildings designed with their first floor not at sidewalk level can significantly reduce the attractiveness of the street to pedestrians. Maintaining and enhancing this pedestrian orientation is essential to the economic and social well-being of the Core Area.

Implementation: A. Incorporate into Core Area design review guidelines that the first floor of Core Area buildings shall be designed for pedestrian accessibility and sidewalk orientation.

B. Amend Figure 5: Multilevel Parking Concept in the General Plan, Section 2.5 Core Area, to show building frontage at ground floor level instead of parking. Also, delete the note in this Figure. Replace General Plan Figure 5 with a figure that is consistent with Figure 12 in the Core Area Specific Plan.

- 5. Policy:** When the public plaza is created in the Core Area, office uses shall be restricted in the ground floor of buildings fronting on the plaza.

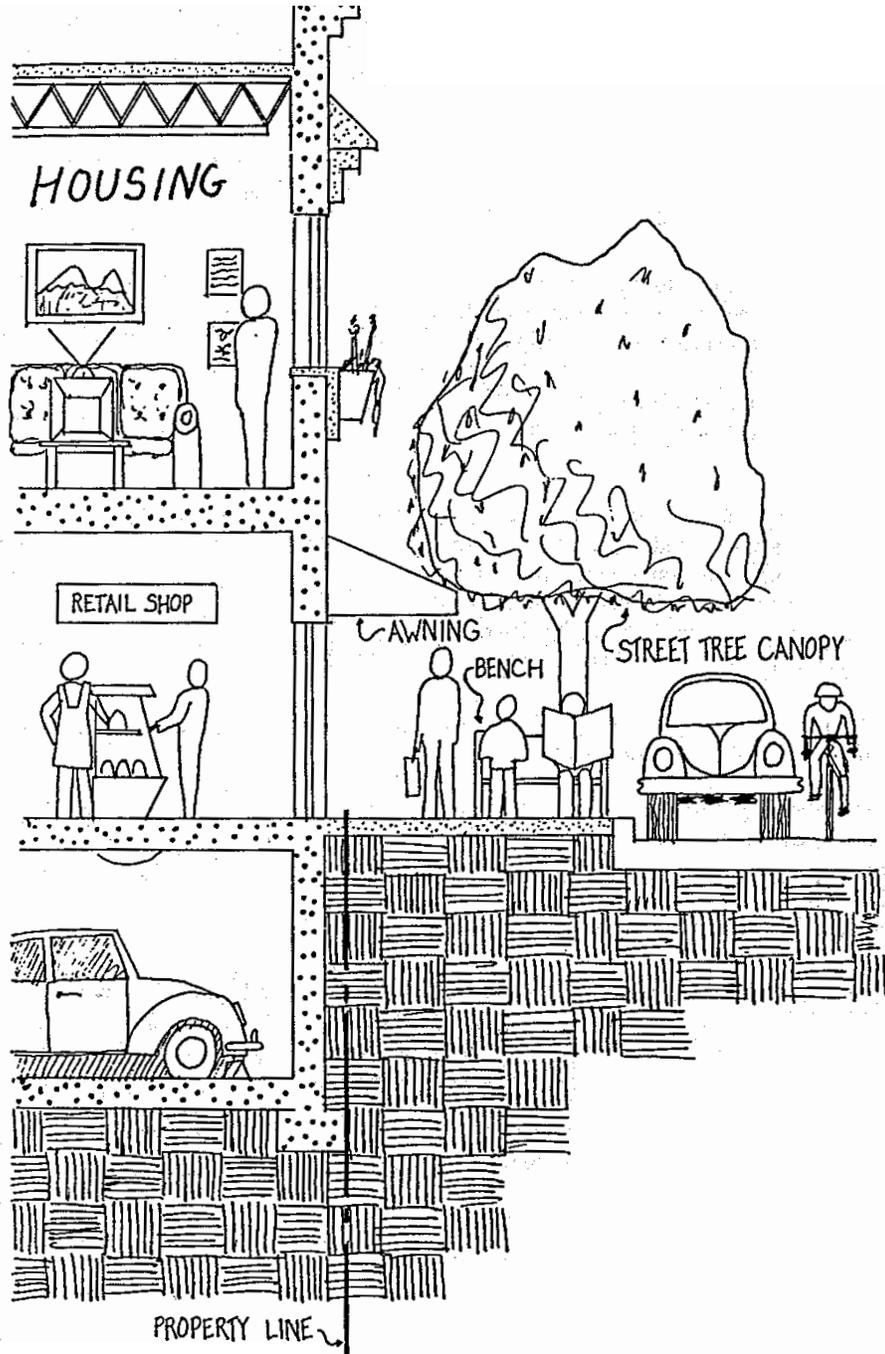
Explanation: The objective is to maximize the use and enjoyment of a public plaza. Retail uses, especially eating and drinking establishments, attract the most people and would be the best foot traffic generators for a major public open space.

Implementation: A. Amend the Zoning Ordinance to restrict office uses on the ground floor of property fronting on the public plaza.

- 6. Policy:** Land use intensification shall be concentrated first in the area bounded by First and Fourth Streets and D Street and the railroad tracks (with an extension north to Fifth Street along F and G Streets); areas to the west and north shall intensify more slowly, with the exception of the large projects discussed in Policy 7.

FIGURE 10

MULTI-USE BUILDING CONCEPT WITH HOUSING



CORE AREA SPECIFIC PLAN

Explanation: The Core Area will grow as Davis grows. It can grow upward (intensify in place) or outward (displacing residential uses on its fringes) or both. The Task Force recommends infill as it results in a more lively (because activity is concentrated) and pedestrian-oriented downtown and maintains valued older housing stock within its edges.

Implementation: **A.** City Planning staff shall inventory buildings in this area and identify one story structures that are suited for second and third story additions. A program to enlarge these buildings and obtain financing from the Redevelopment Agency shall be prepared.

B. All second and third story additions shall be of the proper scale and proportion in relation to the existing and surrounding buildings and to the street. The additions shall continue the architectural theme and context of the existing building while being sensitive to the design of adjacent buildings. The additions shall be designed to be integrated into the existing street fabric so that they become integral parts of the neighborhood. In some instances it may be necessary to step back the second or third story addition in order to achieve the appropriate human scale and to create a transition from smaller to taller buildings.

When projects are submitted to the City, the applicant shall provide streetscape elevations and drawings that indicate the height, setbacks, window proportions, decorative elements, style, entryway placement, types of materials, colors, and landscaping to be used not only for the proposed project but also of the adjacent buildings. This is in addition to the other application submittal requirements. This information will assist the applicant and City staff in determining the appropriateness of the proposed project and how it contributes to unifying and enhancing the street and neighborhood.

C. An alternative or supplementary means of implementing this policy would be to allow a higher floor area ratio in this area than elsewhere in the Core.

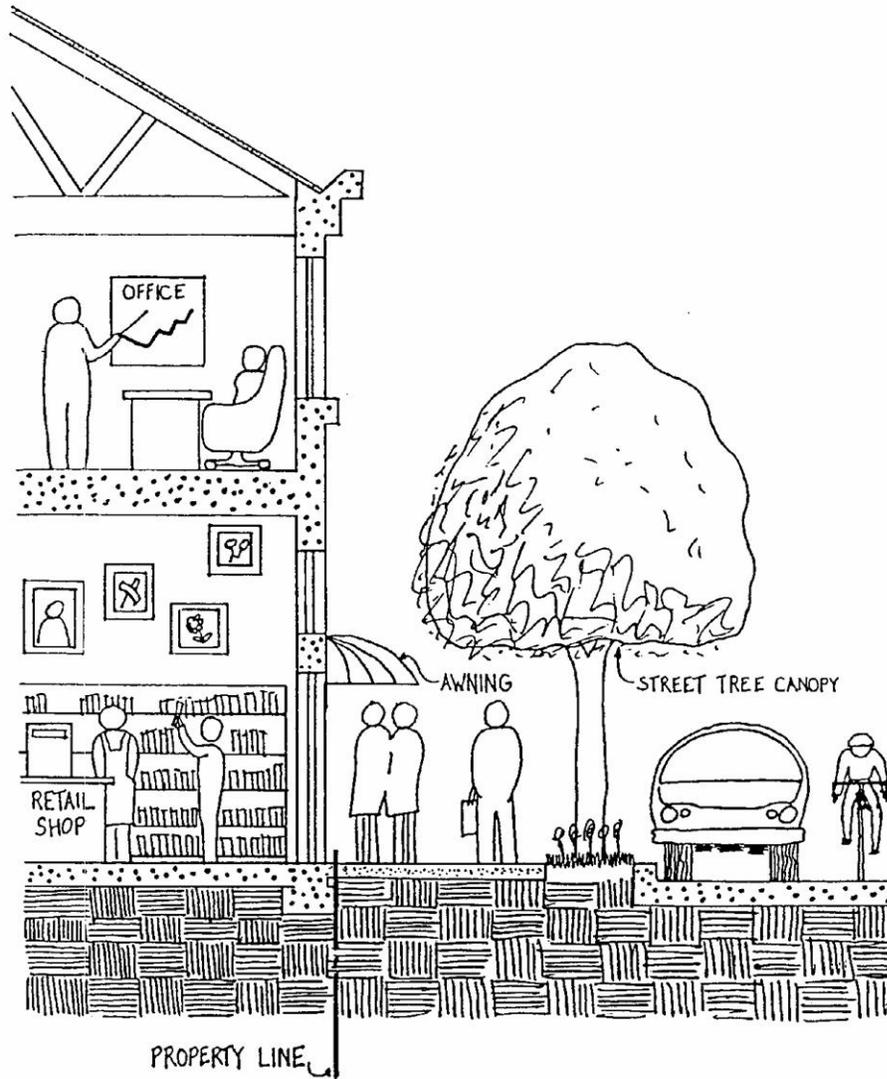
- 7. Policy:** Ultimately the Core Area should be anchored by relatively large developments, that are of an appropriate scale and character at Fifth and G Streets and Aggie Village. In addition, other long term opportunities include intensification at the F Street Parking lot north of 3rd Street and at Third Street near the Central Park expansion. These projects shall incorporate on-site parking facilities that are consistent with the requirements and standards set forth in the Core Area Specific Plan.

Explanation: Downtown Davis can strengthen its retail base and still retain its small city character if larger new developments are located appropriately. The Aggie Village and Fifth and G Street sites are on the edges of the Core Area and will provide substantial new retail space. Of the remaining identified development locations, the F Street site is a higher priority and likely to develop sooner because of its ownership, configuration, and proximity to the core of the core. The suggestion for a new retail cluster near the Central Park expansion is rooted in the desire to strengthen the Third Street link between the University and the Core Area and to encourage use of the new park facilities, including the Teen Center.

These major new developments can contribute to the Davis economy by providing shopping opportunities in the City's areas of deficiency, such as apparel, general merchandise, and home furnishings.

FIGURE 11
MULTI-USE BUILDING CONCEPT

FIGURE 11
MULTI-USE BUILDING CONCEPT



CORE AREA SPECIFIC PLAN

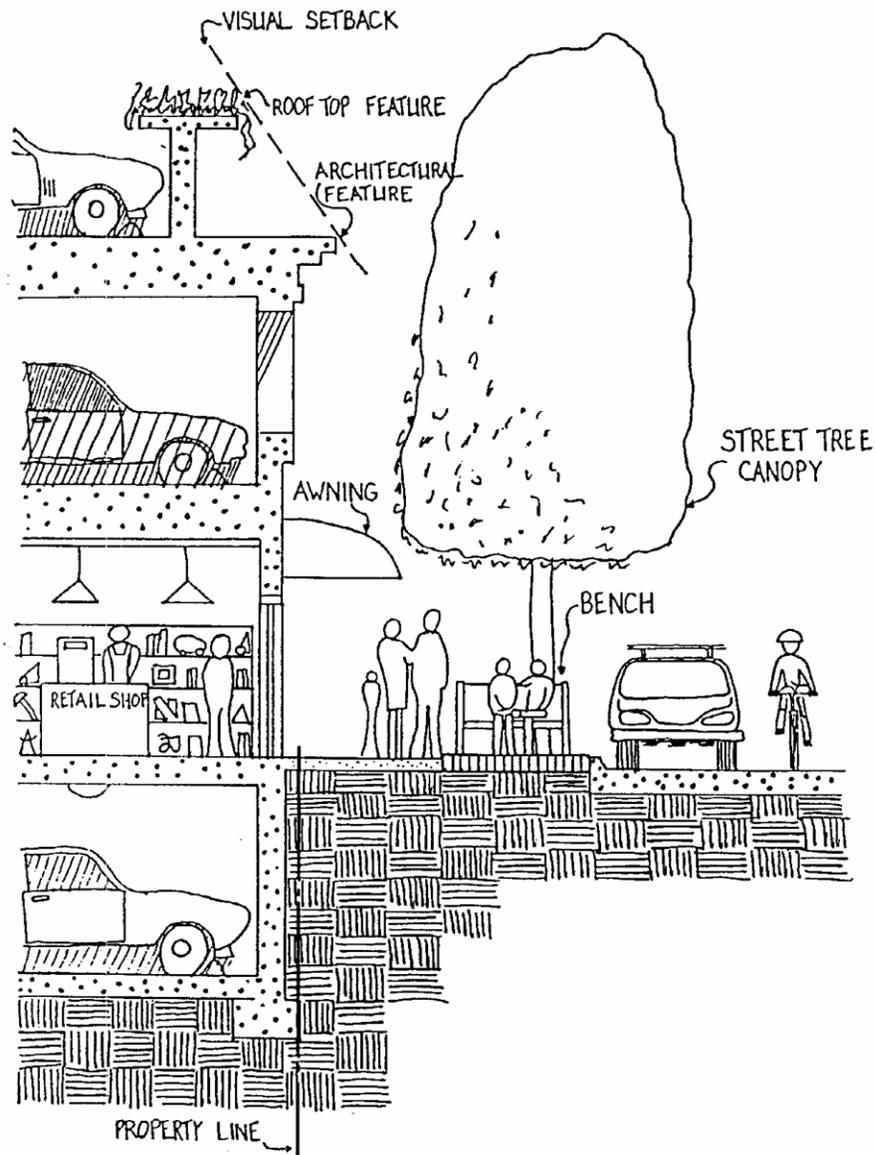
Implementation: **A.** City Planning staff should encourage property owners to facilitate these new developments; Redevelopment Agency powers may be used in the case of the proposed Third Street development. By targeting the Third Street area for the development of a new retail cluster, assembly of parcels into major units by the Redevelopment Agency shall be avoided elsewhere in the Core.

B. The area along Third Street shall be treated with sensitivity because of potential impacts on adjacent land uses. Development along this corridor shall be of an appropriate scale and character in relation to the surrounding and adjacent land uses. The design of projects in this area shall also be sensitive to the Core Area as a whole. Particular attention needs to be given to pedestrian, bicycle, and automobile circulation, and to parking. On-site parking facilities shall be incorporated into these projects that are consistent with the requirements and standards set forth in the Core Area Specific Plan (Figures 12 and 16), Design Guidelines and Zoning.

C. Care shall be taken in developing the Aggie Village and the Fifth and G Street sites. These developments have the potential for impacting the Core Area. Sensitivity shall be given to the design, layout and development of these sites. These projects shall be designed in context, that is, they shall provide enough visual linkages between the proposed projects and adjacent and surrounding existing buildings in the Core Area so as to create a cohesive overall effect. These projects shall strengthen and enhance the characteristics of the Core Area or at least maintain key unifying effects. These projects shall respect the positive qualities of the existing surrounding environment, both natural and man made, and the scale and character of the Core Area as a whole. Pedestrian, bicycle, and automobile access and circulation and parking are critical and shall be viewed in context of the whole Core Area. On-site parking facilities shall be incorporated into these projects that are consistent with the requirements and standards set forth in the Core Area Specific Plan (Figures 12 and 16).

D. Although not included as part of the Core Area Specific Plan study area, care also needs to be given to the development of the Gateway project. This project has the potential for impacting the Core Area. The Gateway project design shall take the Core Area into consideration in relation to scale, character, and pedestrian, bicycle, and automobile access and circulation.

FIGURE 12
MULTI-LEVEL PARKING CONCEPT



SECTION III. CIRCULATION

3.1 AUTOMOBILE CIRCULATION

Automobile and pedestrian travel are often at odds. The main purpose of the circulation measures in the Core Area Specific Plan is to direct automobile traffic around the Core Area, and to maintain a pedestrian priority within the Core (Figure 13). The character of the streets greatly impacts the character and atmosphere of the areas they pass through and connect. Streets must be recognized as part of, and integrated into, the total environment of the Core Area.

1. **Policy:** With the exception of changes to Richards Boulevard, the underpass, and First Street, the existing Core Area street and bike path grid shall not be altered. Short left turn lanes at some corners may be appropriate, but no additional four-lane streets shall be developed.

Explanation: This is a key concept for maintaining a high-quality pedestrian environment in the Core Area. More automobile traffic in the Core will alter the balance away from pedestrians, making sidewalks and outdoor areas noisier, and interfering with easy pedestrian movement. The goal should not be to make it easier to drive a car (i.e. widening streets, etc.). The Core Area must be a place to go to, not through. Through traffic is to be discouraged and shall be limited to certain streets on the edges of the Core Area. The remaining inner streets will function as "main street" type roads for destination traffic. There needs to be identifiable circulation patterns, that is, streets need to be recognized as mainly for pedestrian, bicycle, transit and/or automobile traffic.

The Revised Open Space Element (adopted June 27, 1990) of the City's General Plan has designated First, Third, Fifth, A, B and F Streets and Richards Boulevard as Greenstreets. The primary goal of Greenstreets is to provide convenient and attractive circulation routes for bicyclists and pedestrians, as well as for cars. These circulation corridors are to promote non-motorized transportation. The Greenstreets are to have adequate tree canopies.

Implementation: A. Amend the Major Projects Financing Plan and smaller-scale Public Works improvements to be consistent with the Core Area Specific Plan.

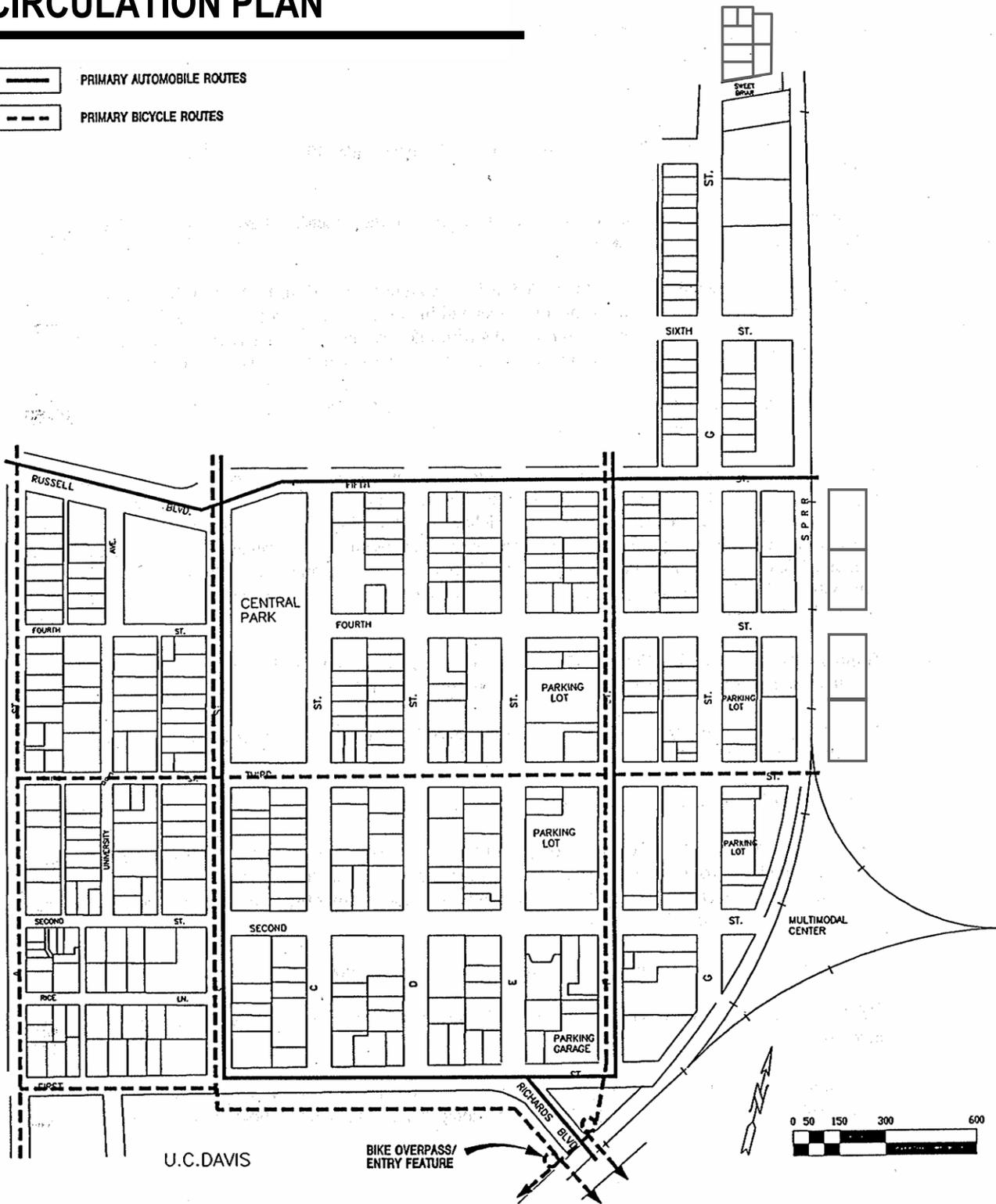
B. The City shall continue to work with UC Davis in developing and implementing Transportation Systems Management (TSM) programs for university employees and students.

C. Circulation patterns shall be made identifiable for pedestrian, bicycle, transit, or automobile traffic. This shall be accomplished in part by the use of distinct landscaping or lighting themes. Landscaping themes shall incorporate several varieties of trees along blocks. New street landscaping shall incorporate any distinctive elements of the existing landscaping. Street tree plantings shall be subject to the review and approval of the Parks and Community Services Department.

D. The primary automobile through routes have been identified as First, Fifth, B and F Streets (Figure 13). These streets can accommodate limited through traffic. All remaining inner destination core area streets shall function as the primary pedestrian routes. The primary bicycle routes have been identified as

FIGURE 13
CIRCULATION PLAN

-  PRIMARY AUTOMOBILE ROUTES
-  PRIMARY BICYCLE ROUTES



CORE AREA SPECIFIC PLAN

A, B, F and Third Streets. The primary transit routes have been identified as A, B, F, First, Second, Third and Fifth Streets (Figure 15).

E. Infrastructure projects in the Core Area shall incorporate the Greenstreet goals and the guidelines for existing Greenstreets into their design.

F. The City shall establish a Core Area "Pedestrian Awareness" month to be held during a fair weather time of year. The programs during this month should include: free day bus passes, shuttle service, street fairs, and sidewalk sales. There should be booths with information on alternative modes of transportation that are available to Davis residents and on various environmentally related issues, such as air quality.

- 2. Policy:** Improvements to the First Street/Richards Boulevard corridor shall be determined by a plan designed specifically to address that issue.

Explanation: Recognizing that there are differing viewpoints regarding the effects of increasing vehicle capacity in the First Street/Richards Boulevard corridor, traffic analyses show that capacity improvements are needed in this corridor. As detailed in the Richards Boulevard Corridor Upgrade EIR, this conclusion holds true under the most optimistic of assumptions concerning the diversion of university traffic, enhanced demand measures, completion of the Pole Line overcrossing and Mace improvements, and other alternatives that have been suggested. Using the established policy for roadway level of service, the project is warranted.

Implementation: A. The Richards Boulevard corridor shall be improved consistent with the detailed design plan approved by the City and consistent with the General Plan.

B. The final lane configuration of First Street shall be consistent with the adopted plan for the Richards Boulevard undercrossing.

- 3. Policy:** Fifth Street shall not be widened.

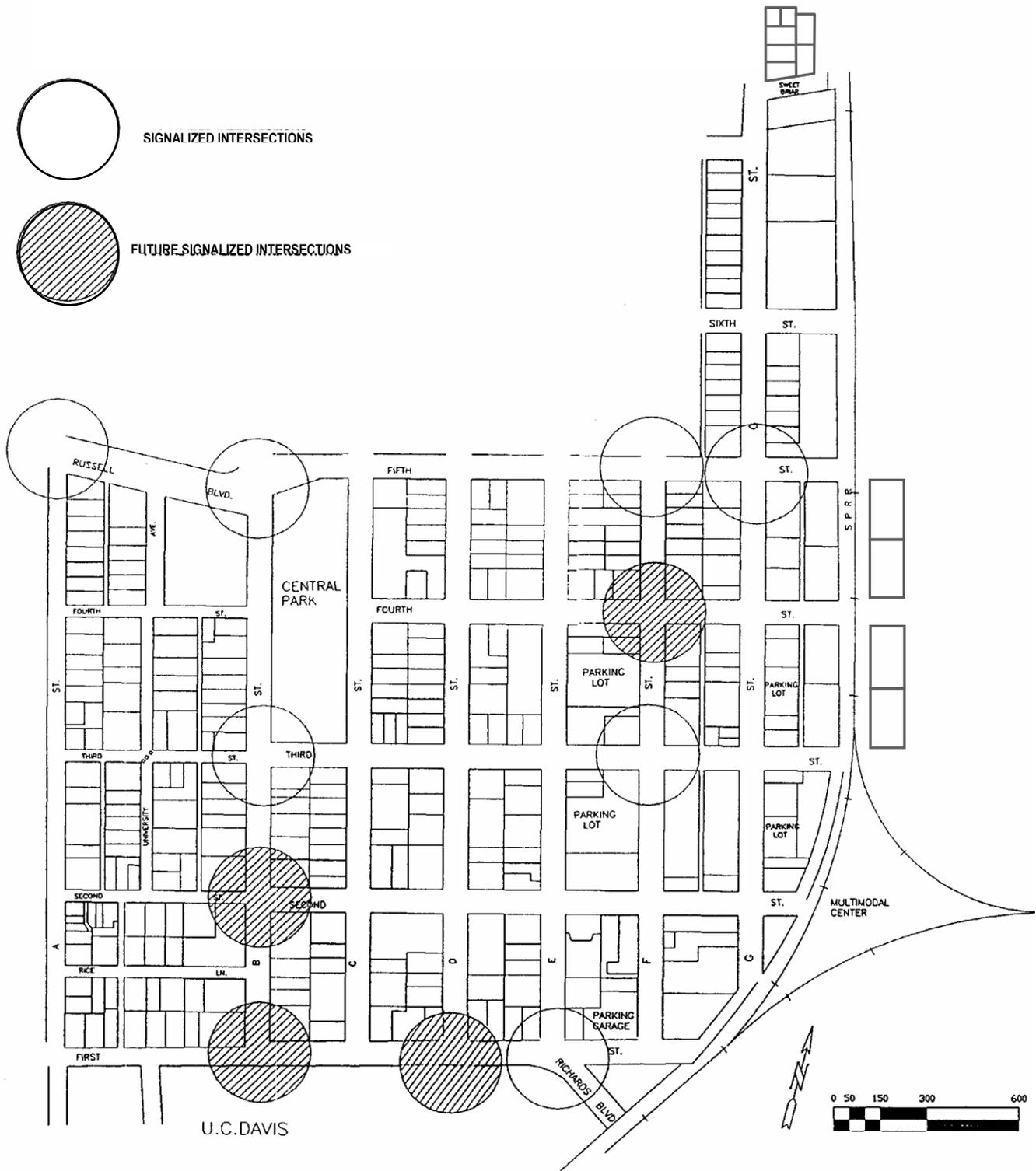
Explanation: Additional traffic lanes along Fifth Street will require the removal of over 100 street trees between A and L Streets. This impact is unacceptable and is inconsistent with the designation of Fifth Street as a Greenstreet. Bicycle traffic shall be directed to the existing through bike lanes on Eighth and Third Streets, not along this busy arterial.

Implementation: A. Maintain Fifth Street in its current configuration.

- 4. Policy:** Limit the installation of new traffic signals in the Core Area to streets designated primary automobile routes (Figure 13).

Explanation: Traffic signals facilitate the flow of automobile traffic at the expense of pedestrian movement. This tradeoff is not acceptable except along routes designated to move traffic around the Core Area.

FIGURE 14
SIGNALIZED INTERSECTION PLAN



Implementation: A. The signal installation program shall conform to the Core Area Specific Plan. Traffic signals shall be pedestrian friendly.

B. Traffic signals are permitted at the following Core Area intersections: Fourth and F, First and B, First and D, and Second and B (Figure 14).

C. Study the need for traffic signals at the following intersections: First and F, First and C, and Second and F.

3.2 TRANSIT

Davis enjoys some of the best transit service in Northern California. Ridership on the systems serving Davis is increasing, and even better service is now possible with increased financial support through Propositions 108 and 111, and a student referendum. Improving transit service to the downtown will increase activity in the Core Area without generating more automobile traffic and the need for more parking (Figure 15).

1. Policy: When light rail is extended to Davis, a station should be located in the vicinity of the existing SP depot.

Explanation: Adding light rail service to those transportation options already existing at the multimodal center will further improve transportation service and will help maintain the Core Area as the focus of Davis.

Implementation: A. Continue to support cooperative efforts to bring light rail into Yolo County and Davis and include this policy in any regional transportation plan for the Davis area.

2. Policy: Study the concept of a transit-only southbound lane on A Street to improve Unitrans circulation.

Explanation: Two-way transit service on A Street, with automobiles allowed only to travel in a northbound lane, may improve UC Davis oriented transit service and facilitate transit access to the Core Area.

Implementation: A. Cooperate with Unitrans and UC Davis to study this important transit linkage to the Core Area.

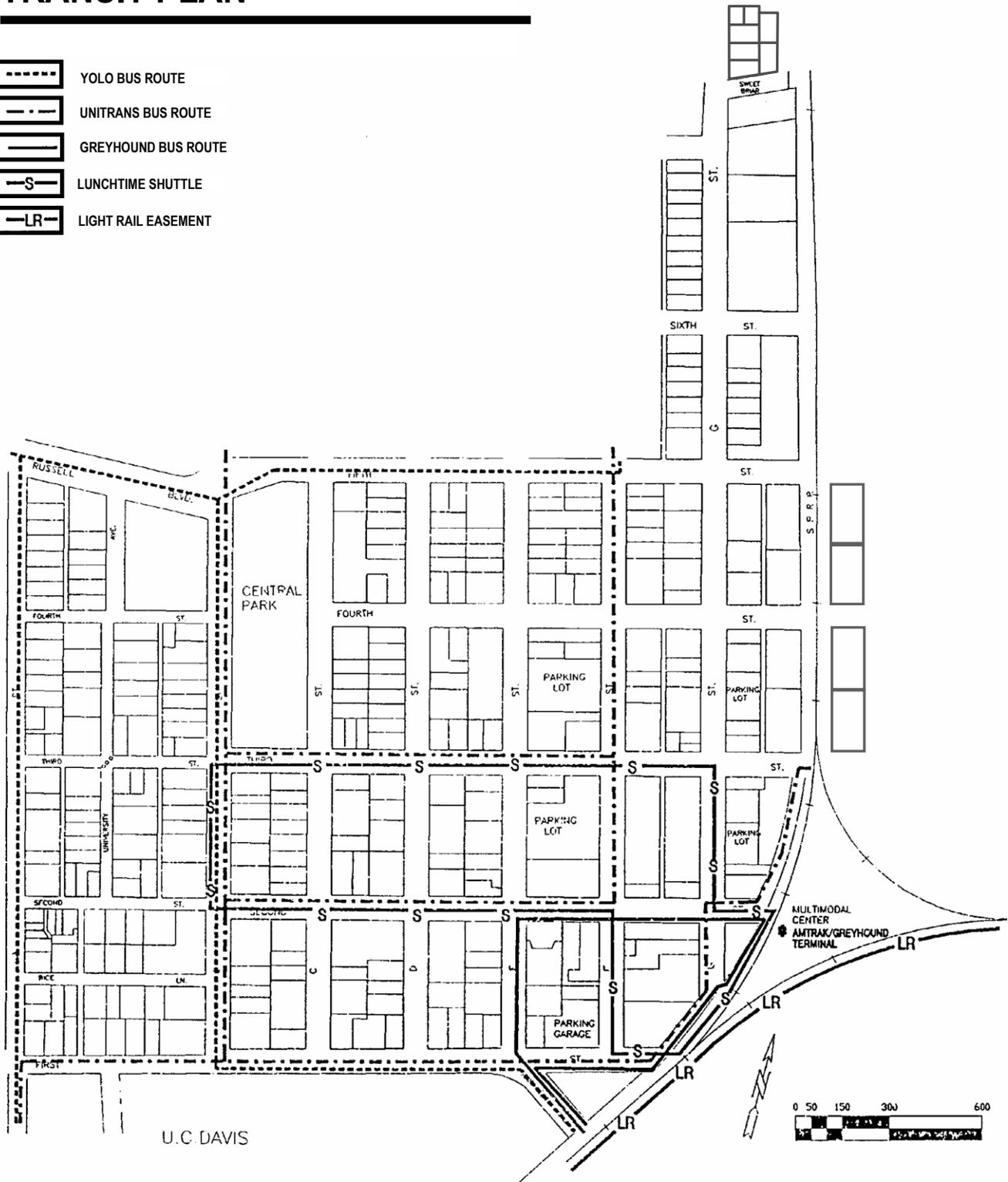
3. Policy: Additional permanent revenue sources to support transit need to be identified.

Explanation: Transit access to downtown is an important way to reduce parking needs. Support of this service with funding sources such as in-lieu parking fees or parking fines shall be considered.

Implementation: A. Cooperate with UC Davis, Unitrans, and Yolobus to study and develop methods to provide additional permanent support for transit services to the Core Area.

FIGURE 15
TRANSIT PLAN

-  YOLO BUS ROUTE
-  UNITRANS BUS ROUTE
-  GREYHOUND BUS ROUTE
-  LUNCHTIME SHUTTLE
-  LIGHT RAIL EASEMENT



3.3 BICYCLES

It is well known that an extensive bike path network is available to Davis residents. Bicycle connections and bicycle parking shall be improved in the Core Area to further encourage the use of this alternative mode of transportation (Figure 13).

1. **Policy:** Bicycle traffic from South Davis to the University should be routed across a separate SP crossing near Putah Creek.

Explanation: The interface between bicycle and automobile traffic in the Richards Boulevard area is complicated and frustrating to rider and driver alike. The City and the University have agreed that separate bicycle access to UC Davis will benefit both.

Implementation: A. Design and construct separate bicycle access to UC Davis consistent with the Core Area Specific Plan.

2. **Policy:** East-west bicycle traffic shall be routed to Third and Eighth Streets.

Explanation: Existing bike lanes along Third and Eighth Streets provide access to UC Davis, the Core Area, and East Davis.

Implementation: A. Maintain bike lanes on Third and Eighth Streets.

B. Maintain Fifth Street in its current configuration, except for turn lanes at F and G Streets.

3. **Policy:** Complete the bike lane grid providing access to the Core Area.

Explanation: Bicycle access to the Core Area is an important alternative to automobile travel and reduces the need for costly parking structures.

Implementation: A. Conduct a study to examine ways to improve bicycle circulation by linking the existing terminus of the F Street bike lane at Third Street with the proposed bike lane at First and F Streets, and extending the A Street bike lane south from Third to First Streets.

B. Provide adequate bicycle parking in the Core Area. The Public Works Department shall survey the status of bicycle parking in the Core Area. Public Works shall develop a standard plan including policies, design, style, location, and number of bicycle racks for the Core Area which is consistent with the Core Area Specific Plan. When this standard plan is finalized, it shall be incorporated by reference into the Core Area Specific Plan.

C. Keep freestanding bike lane and bike path signage to a minimum.

3.4 PARKING

Parking spaces are a valuable commodity. They are in limited supply on-street, and cost up to \$20,000 each to provide in off-street structures. The measures directed at parking are intended to maintain the current supply of on-street spaces and to see that they are put to their best use.

1. **Policy:** Avoid creating public parking structures as long as viable alternatives exist.

Explanation: Before more public funds are invested in parking structures, attention shall be given to alternatives including more supply (through better enforcement and reestablishing recently eliminated on-street parking spaces except those in the University Avenue Area), and reduced demand (through better transit access). Additionally, the SACOG Air Quality Plan for the Region specifically targets parking structures as impairments to clean air and maintains that local governments, including Davis, should prohibit them.

Implementation: A. Do not construct additional large parking structures unless they are related to specific approved development projects and only if it has been determined that no viable alternative exists for the provision of the project's parking. Viable alternatives include, but are not limited to, the provision of an adequate number of bicycle racks in all developments except single-family residences; private subsidies for public transportation; improved bicycle lanes; dwelling units built near destination points; and, allowing developers and businesses to substitute a portion of any required parking with other transportation management measures such as ride-sharing programs and providing employees with transit passes or bus tokens. This last alternative shall be monitored to ensure compliance with the measures.

B. Project applications shall include proposals for mitigating traffic and parking generated by the project. Density shall be linked to the traffic and parking generated by the project. Parking requirements may be reduced if the applicant provides implementable TSM measures; the project shall be monitored for compliance with the measures. If the project is in non-compliance, the developer may be required to provide additional subsidies to transit/ride sharing programs.

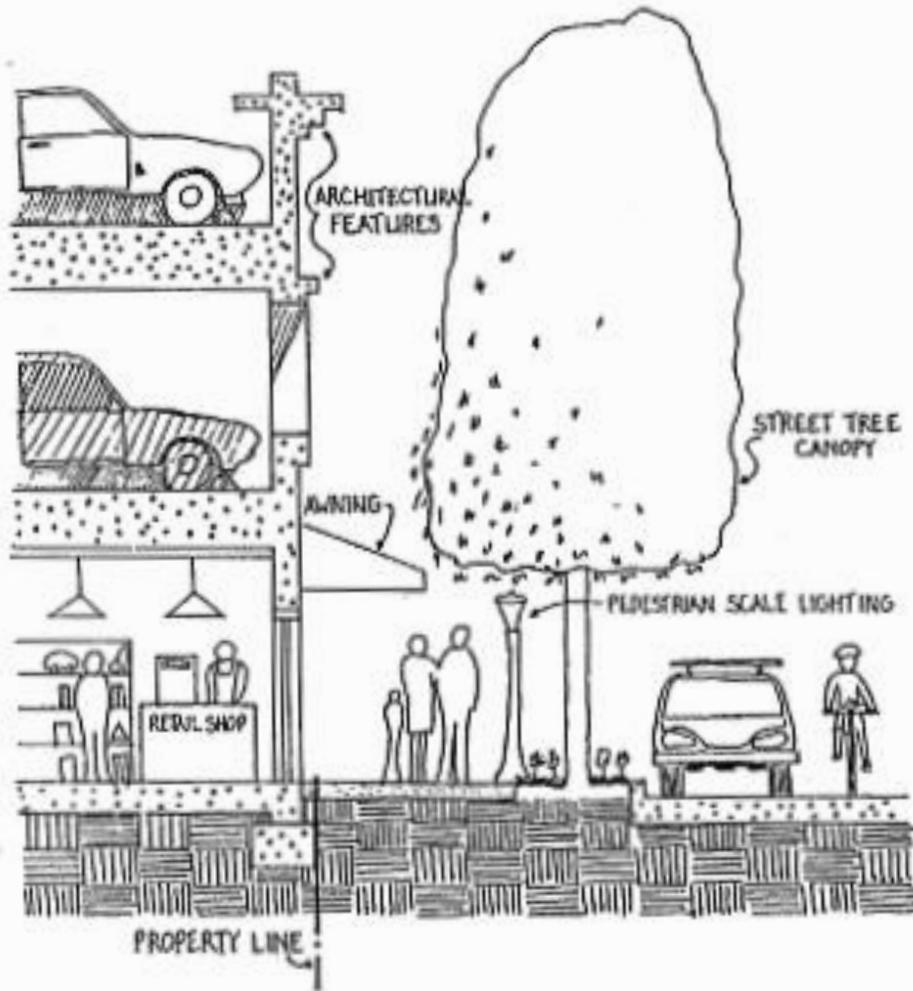
C. Planning staff shall review the City's minimum parking requirements to determine if they reflect actual parking demands and revise the Zoning Ordinance accordingly.

D. Increase the on-street parking supply by reestablishing parking spaces that have been eliminated (not including those in the University Avenue Area), to facilitate turning at intersections and by enforcing parking limits.

2. **Policy:** Future large-scale parking structures shall be located at the periphery of the Downtown Core (Retail Stores) area and designed, whenever possible, to provide retail frontage along sidewalks (Figures 10, 12, and 16).

Explanation: Large-scale parking structures shall not be built in the Downtown Core because the inevitable increase in automobile traffic entering and leaving the structures can adversely impact the pedestrian, bicycle, and automobile circulation system.

FIGURE 16
MULTI-LEVEL PARKING CONCEPT



CORE AREA SPECIFIC PLAN

Implementation: A. If it is determined through the on-going downtown parking study that additional parking structures are necessary, they shall be built at the periphery of the Downtown Core (Retail Stores) area. Parking structures shall be built whenever possible with street-level retail space to eliminate "dead spaces" along pedestrian ways.

B. Parking shall be provided in a way that is consistent with the Core Area Specific Plan.

C. The City and downtown merchants shall study the possibility of establishing a Core Area shuttle service. The use of a shuttle service will enable people to utilize parking lots on the periphery of the Core Area and to shuttle to their destination in the Core Area. To be successful, the shuttle service will need to run on a regular schedule, preferably every half hour, six to seven days a week. This shuttle service program might include renting parking spaces in UC Davis parking lots.

D. Amend Figure 5: Multilevel Parking Concept in the General Plan, Section 2.5 Core Area, to show building frontage at ground floor level instead of parking. Also, delete the note in this Figure. Replace the General Plan Figure 5 with a figure that is consistent with Figures 10, 12 and 16 in the Core Area Specific Plan.

- 3. Policy:** Increase the availability of on-street parking spaces for short-term visitors to the Core Area.

Explanation: This measure is intended to free up convenient on-street parking for the use of Core Area shoppers. Recent studies have indicated that a majority of on-street parking spaces in the Core Area are taken up by long-term parking. This places Core Area merchants at a disadvantage when compared with peripheral retail centers with plentiful free parking.

Implementation: A. A study should be conducted to determine the feasibility of making both levels of the parking structure located at First and F Street free long term parking directed at Core Area employees.

- 4. Policy:** Improve signage for existing off-street parking spaces.

Explanation: On-street parking is obvious and close at hand. Improved signage will increase awareness of off-street parking and can result in better utilization of off-street parking facilities.

Implementation: A. Develop a sign program to publicize the availability of off-street parking in the Core Area.

B. Print brochures showing the location of Core Area parking facilities and make these brochures available at prime locations.

- 5. Policy:** Surface parking spaces removed to provide a public plaza shall be replaced at or near the plaza site.

Explanation: A public plaza will be a wonderful asset to the Core Area, but should not be provided at the expense of existing parking that shoppers and merchants have come to rely on.

Implementation: A. Develop replacement parking supplies if spaces are lost due to the creation of a public plaza.

6. **Policy:** Use parking fines to restrict long-term use of on-street parking in the Core Area.

Explanation: Common knowledge and Police Department statistics confirm that over one-half of the available on-street parking in the Core Area is taken up by individuals who shuttle cars from space to space all day long. Many alternatives are available to these employees, including parking in peripheral neighborhoods, biking, transit, and the parking structure at First and F Streets.

Implementation: A. Increase on-street parking enforcement (computer assisted) to free up spaces for shoppers. Escalating parking ticket fees for repeated violations shall be considered.

7. **Policy:** In-lieu fees for parking shall be set at a level that reflects the true cost of providing substitute underground or structure parking. In-lieu fees should be spent within ten years of collection.

Explanation: In-lieu parking fees can improve the nature of development by reducing the number of driveways and the portion of a parcel devoted to parking (see Figure 29 for the established Core Area Parking Districts). This keeps development dense and improves pedestrian access. Developers should have some assurance that necessary parking will be provided in a timely manner, however, or that alternative access will be provided.

Implementation: A. Use in-lieu parking fees to provide new parking supplies or to reduce the demand for parking. Providing access by having in-lieu fund expenditures support alternative transportation instead of additional parking shall be considered. Small-scale developments in the Core shall be encouraged to pay in-lieu fees, rather than provide on-site parking, to reduce the number of driveways in each block face.

8. **Policy:** Fully utilize curbside parking potential in the Core Area.

Explanation: Each pair of left turn lanes removes 15 to 20 parking spaces, and each right turn lane or area removes at least one parking space. If Core Area parking supplies are to be maintained, spaces removed for turn lanes must then be replaced at high cost. Put into perspective, the pair of left turn lanes added at Fourth and F Streets eliminated \$200,000 worth of on-street parking. Within the Core Area, it is not acceptable to eliminate valuable parking spaces in order to facilitate turning movements.

Implementation: A. Except for left turn lanes on B and F Streets, left and right turn lanes shall not be created in the Core Area due to their impact on curbside parking spaces. Corner parking spaces removed recently at some intersections to facilitate right turn movements shall be evaluated for reinstatement.

3.5 MISCELLANEOUS

1. **Policy:** Continue to restrict large truck travel in the Core Area, except as absolutely necessary for deliveries.

Explanation: Large truck traffic in the Core Area shall be minimized as it impairs automobile and pedestrian circulation and reduces air quality.

Implementation: A. Establish truck routes through the city which minimize large truck traffic through the Core Area.

B. Study large truck deliveries to the Core Area and, if necessary, limit delivery times for large trucks to the Core Area.

2. **Policy:** Improve pedestrian access between UC Davis and the Core Area.

Explanation: Delivery trucks, parked cars, and circulating cars all impede pedestrian access along Third Street near University Avenue. Improving pedestrian access will provide an important link along Third Street between the University and the heart of the Core Area.

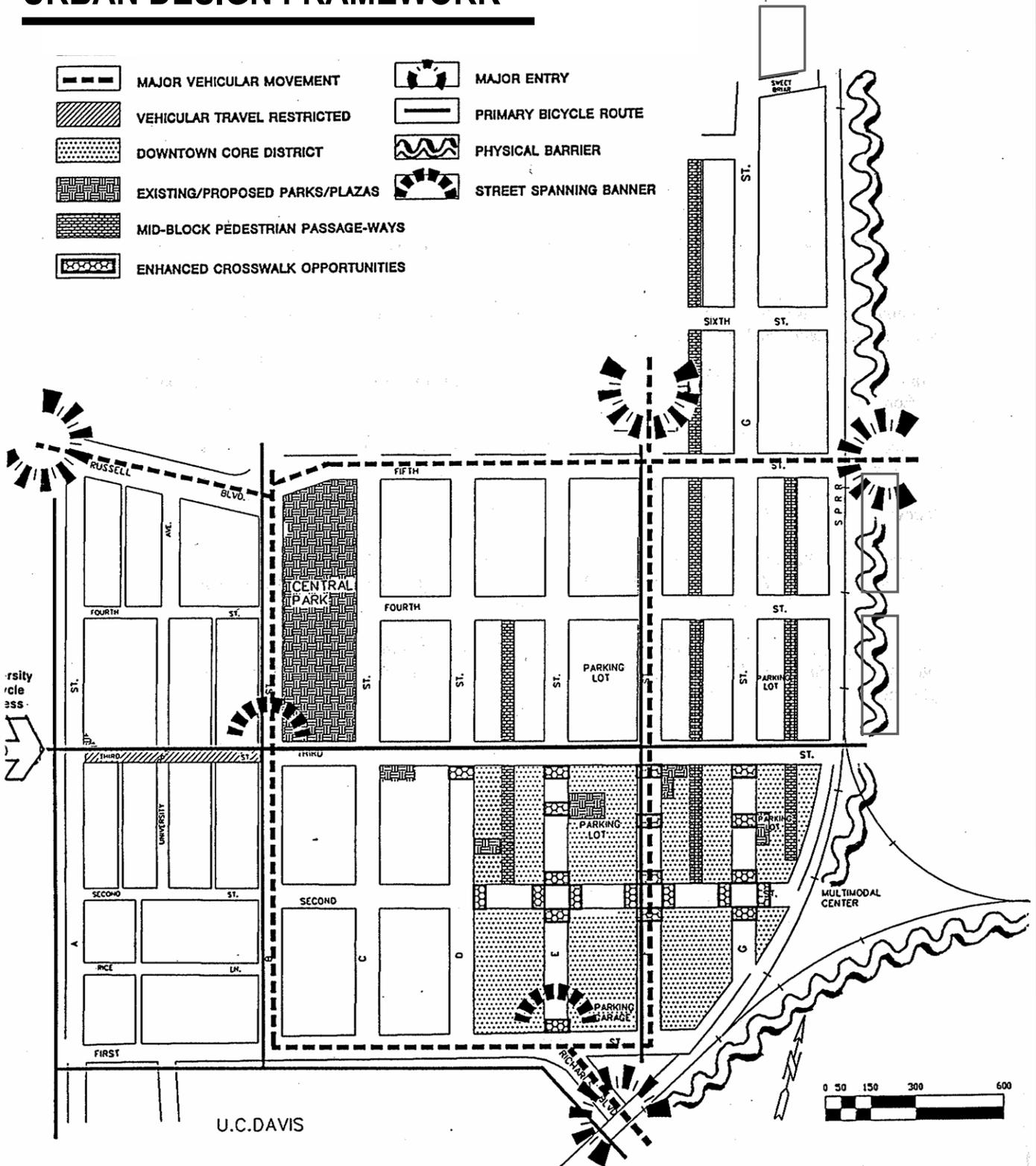
Implementation: A. Existing bollards shall be maintained on Third Street to discourage through automobile circulation on Third Street and University Avenue while facilitating walking and biking between UC Davis and the Core Area.

B. Initiate a street improvement program for 3rd Street between A and B Streets including consideration of widening the sidewalk, installation of new street lighting, street furniture and tree grates, replacement of unhealthy trees, possible undergrounding of utilities, enhanced pedestrian crosswalks and modification of street paving and design to formalize a multiple use street.

FIGURE 17

URBAN DESIGN FRAMEWORK

-  MAJOR VEHICULAR MOVEMENT
-  VEHICULAR TRAVEL RESTRICTED
-  DOWNTOWN CORE DISTRICT
-  EXISTING/PROPOSED PARKS/PLAZAS
-  MID-BLOCK PEDESTRIAN PASSAGE-WAYS
-  ENHANCED CROSSWALK OPPORTUNITIES
-  MAJOR ENTRY
-  PRIMARY BICYCLE ROUTE
-  PHYSICAL BARRIER
-  STREET SPANNING BANNER



SECTION IV. STREETScape

4.1 INTRODUCTION

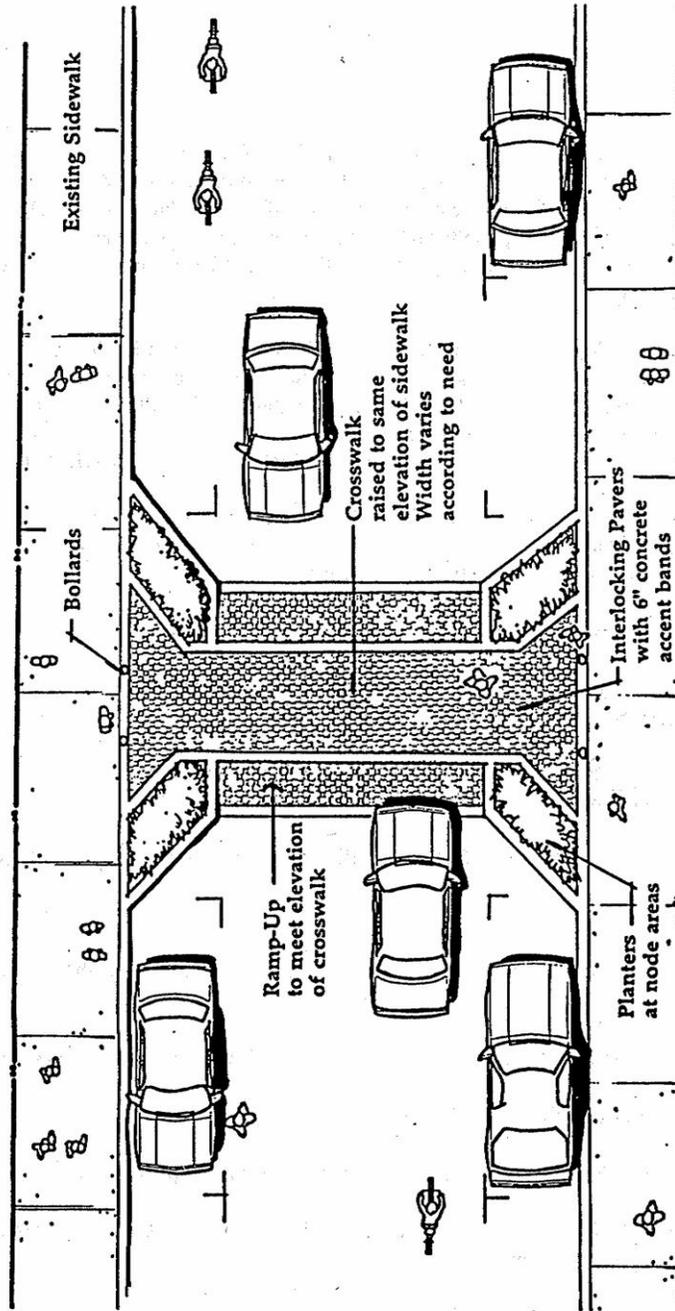
The streetscape section of this plan shall be used to unify the Core Area environment. Any streetscape design program shall be done in a comprehensive manner by a multi-disciplinary team of City planners, professional architects and urban designers, merchants, residents, City Parks and Public Works personnel. Public spaces shall be designed in such a manner that everyone, adults and children, can use and enjoy them. The Core Area can, and should, be a place of discovery and adventure for people of all ages and backgrounds. These common areas can help renew a sense of community.

The City's Landmark Trees and Trees of Significance lists include many trees in the Core Area that are to be protected and preserved. These two lists, and any updates to them, are incorporated by reference into the Core Area Specific Plan and are included in Appendices 9 and 10 of this Specific Plan. The City also has a list of Designated Historical Resources that includes buildings in the Core Area. This list, and any updates to it, are incorporated by reference into the Core Area Specific Plan and is included as Appendix 12 of this Specific Plan.

An Urban Design Framework Plan has been completed for the Core Area. The purpose of such a plan is to combine recommended urban design mechanisms and background information onto a singular policy direction graphic. The graphic (Figure 17), depicts the following information:

- Major Vehicular Movement - These streets can accommodate limited through traffic.
- Street Restricted to Vehicles - Vehicular traffic should be restricted on Third Street between A and B Streets to encourage a pedestrian-bicycle friendly streetscape.
- Mid-Block Pedestrian Passageways - These passageways are designed to enhance the pedestrian character of the Core Area by creating “interesting shortcuts” to pedestrian destinations.
- Downtown Core District - This area defines the traditional retail commercial heart of the Core.
- Street Spanning Banners - Banners used to promote local civic events and add color to the street scene.
- Enhanced Crosswalk Opportunities - These are crosswalks located in and at the edges of the Downtown Core which will receive enhanced paving treatments.
- Major Entry - A point or node where substantial vehicular traffic enters or exits the Core Area.
- Physical Barrier - Delineates the railroad tracks.
- Primary Bicycle Route - Shows the streets where bicycle traffic is the primary method of transportation.
- Existing/Proposed Parks/Plazas - Depicts where accessible public parks and open space plazas currently exist or where future park/plaza opportunities may be located.

FIGURE 18
MID BLOCK SAFE CROSS



Note: All streetscape improvements must conform with Title 24 (California) and federal Americans with Disabilities Act (ADA) requirements for individuals with disabilities.

4.2 AESTHETIC ELEMENTS

There shall be a streetscape design program developed for the Core Area which shall be done in a comprehensive manner. Street furniture, and any other hardscape features, shall be of a design and made of materials that reflect the architectural heritage and ambiance of the Core Area. The following are elements which shall be considered in the streetscape development of the Core Area:

a. Image Defining Elements

The image of Davis is defined by what people see in its physical character - its buildings, streets, trees, and open spaces - and what they may already know about its present and past history - its social, cultural, and physical development. But these characteristics alone are not sufficient if we want people to gain a full appreciation of the City.

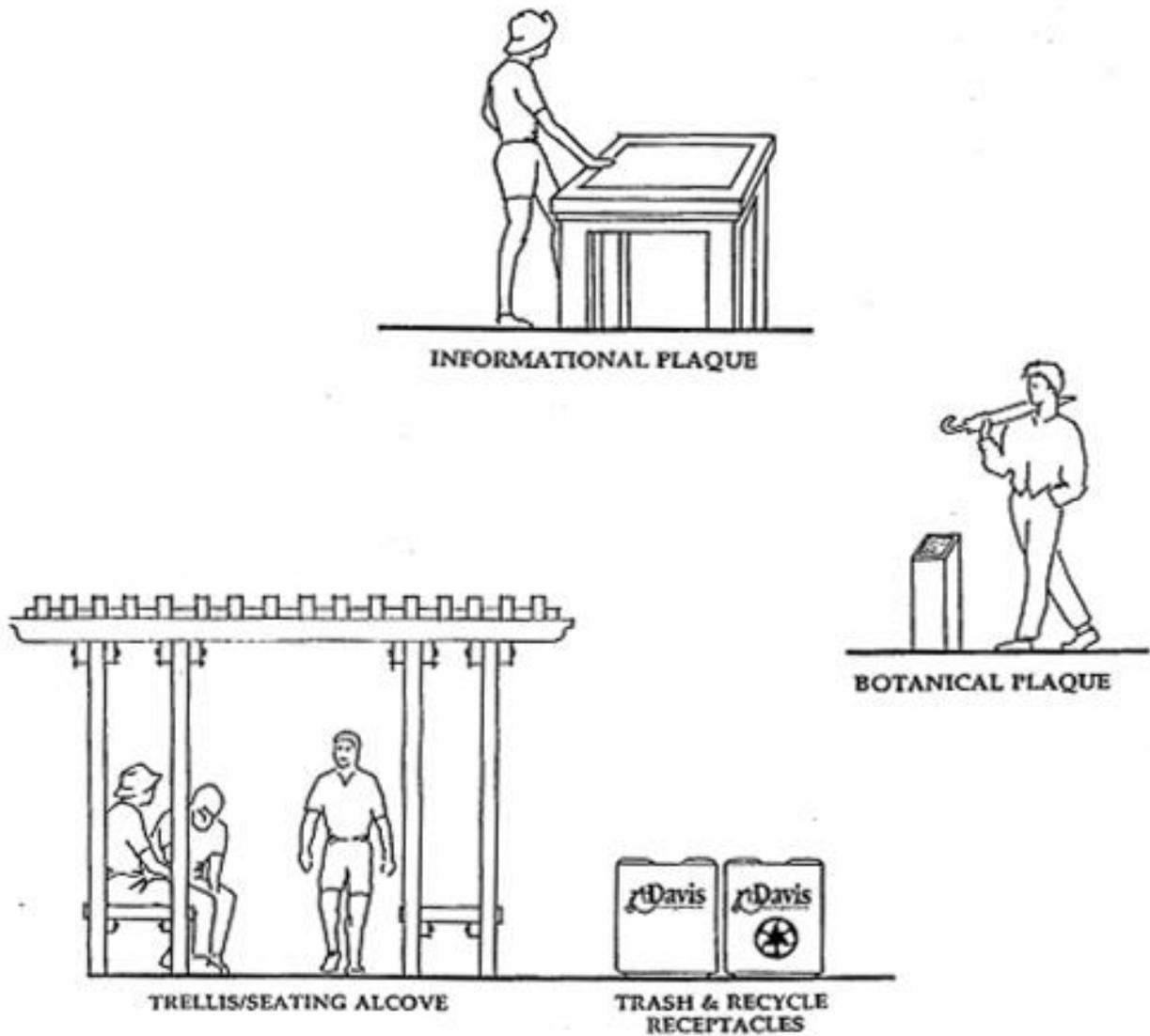
To help visitors and residents alike develop a broader image of Davis today, yesterday and in the future, a program shall be implemented which encourages the development/placement of many "image defining elements" throughout the Core Area. Think of them as "treasures" to discover. They might include a wide variety, but they must be fun, interesting, and accessible to the public. Their accessibility, however, need not be readily obvious and should include elements of "discovery", "excitement" and "surprise." This can be accomplished by locating the elements throughout the Core Area in a seemingly random pattern.

Particular "treasures" might include such items as:

- Functional art elements;
- Two and three dimensional art pieces (refer to Publicly Accessible Art section for more detail);
- Temporary art displays and activities;
- Plaques that provide directions and information about the community;
- Educational kiosks denoting historical events, persons, structures, cultural events, or other facts about the City, UCD and the region; and
- Botanical name plaques identifying significant trees and other vegetation (gardens, wildflowers, etc.)

The City's Parks and Community Services Department, Historical Resources Management Commission and Civic Arts Commission should be consulted when designing plaques. Appropriate locations for these elements can be as varied as the types of elements themselves. And again, it is important to remember that the feelings of "discovery", "excitement" and "surprise" that the observer attains in finding these Civic gems can be as significant as the elements themselves. Appropriate locations can include the following:

FIGURE 18a
PEDESTRIAN AMENITIES (TYPICAL)



- Plazas, both in the public right-of-way and on private property;
- Throughout the "Discovery Walk" trail system;
- Mid-block pedestrian passageways and alleys;
- In front of buildings set back from the sidewalk;
- Public parking lots; and
- Public sidewalks and pedestrian safe crosses (traffic chokes).

The Civic Arts Commission and Historical Resources Management Commission shall work with the Davis Downtown Business Association, Chamber of Commerce, UCD, merchants and private developers, as well as public entities, to provide civic art within their projects.

- **Publicly Accessible Art**

Permanent and temporary publicly accessible art shall be placed throughout the Core Area. The Civic Arts Commission shall work with private developers and public entities to provide art in their projects. Public art is particularly appropriate in public plazas and along the Discovery Walk. Art which celebrates the richness in diversity of Davis should be incorporated into public art projects. When appropriate, public art should be interactive (seating, play, shade, etc.)

Publicly accessible art must strike a balance between responsibilities to a community and to a creative vision. Privately-owned and publicly-owned accessible art enriches the community by:

- Enhancing the social way public spaces function as congenial places;
- Stimulating the community's creativity and encouraging a dialogue on the value of art in every day life;
- Serving as an expression of pride and permanence by ethnic groups proclaiming cultural autonomy;
- Clarifying neighborhood identity and creating a sense of place; and
- Serving functional needs, such as benches and planters, while differentiating from design by injecting meaning as well as amenities into public spaces.

The Core Area should be looked at as a whole to determine the role and placement of public art. The Civic Arts Commission will work with the downtown community to determine the most suitable sites. The following set of criteria, developed by the Seattle Arts Commission for their highly successful public art program, should guide the analysis of a site's suitability:

- Is a site on public property or property readily available to the public art process?
- Does a site suggest art opportunities that would extend the breadth and quality of the Art in Public Places Program? Each recommended site should offer a challenging set of circumstances and allow for a wide range of artistic solutions.
- Will innovative art on the site enhance the downtown pedestrian/streetscape experience?
- Is the site situated in the network of public places? Does the site fall within the prominent paths of circulation (entry point, Discovery Walk, pedestrian corridors, and alleys), or is the site situated near a place of congregation (park, multimodal centers, entertainment and retail centers, etc.)?

Recommendations for site selection from these criteria will fall into the following categories: priority sites, future sites, and temporary sites. The site plan will be implemented by the Civic Arts Commission through the City's Art in Public Places Program and the development of public/private partnerships and incentive programs.

- **Davis Arch Design Competition**

The Davis Arch was built in 1916 at the corner of Second and G Streets, on the north side of Second Street, through the joint efforts of the Women's Improvement Club, Davis Businessmen and Cal Aggie students. The Davis Arch became a symbol of unity between the UC Campus and the Davis community. The Arch was removed in the 1920's due to hazards to increasing automobile traffic.

The Davis Arch is a historical reference that can be brought forward in time to strengthen the tie between the past and the future. Due to the variety of siting and design options for the arch, it is recommended that the City select an artist/sculptor through an open competition. The open art competition could use the Davis Arch as a conceptual starting point, with no predetermined end product, which would allow a wide variety of artistic vision concerning the arch.

b. Pedestrian Pathways

Downtown pathways shall be developed and designed to provide alternate routes and gathering places (Figure 17). Examples aesthetic elements are:

- **Crosswalks**

A crosswalk network encompassing the Core Area and surrounding neighborhoods shall be established. This network shall include crosswalks at all intersections in or adjacent to the Core Area along First, Fifth, B, F, and G Streets. Crosswalks shall be well lit, signed, and clearly striped.

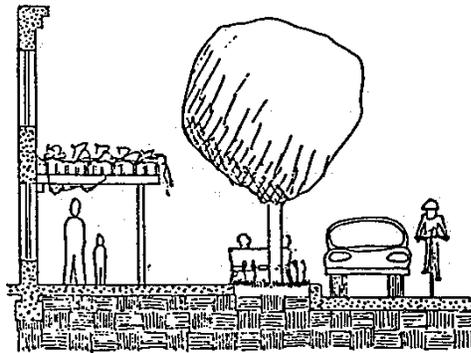
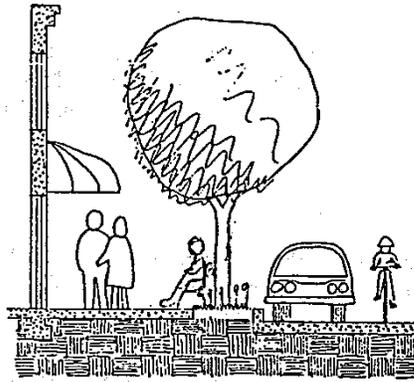
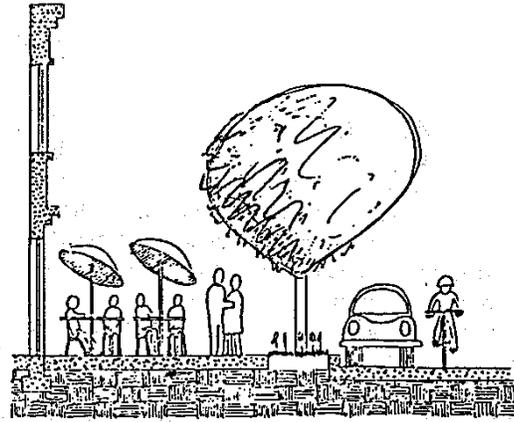
- **Special Enhanced Crosswalks**

Within the Downtown Core Area, all crosswalks shall be enhanced with special paving treatments to increase their visibility for pedestrian safety and to add variety to the visual streetscape. Where the opportunity exists, crosswalks shall be raised to top-of-curb height as shown in Figure 18. The most appropriate paving methods are colored, scored concrete, or colored pavers. The City's Civic Arts Commission could be consulted when a prototype surface treatment is being proposed.

- **Discovery Walk**

A Discovery Walk (pedestrian trail system) shall be developed by the City in cooperation with the Davis Downtown Business Association, Chamber of Commerce, UC Davis, local merchants, residents and City Staff. The Discovery Walk will allow people to connect with the Core Area - to have a sense of the place. The Walk shall be clearly marked and delineated. This pathway will be a scenic route which will take

FIGURE 18b
STREETSCAPE AMENITIES



people on a tour past shops, plazas, public art, historical and cultural resources, Landmark or Trees Worth Saving, and other points of interest.

The Discovery Walk can create a pedestrian passage that links the Core Area's mid-block pedestrian passageways, plazas, parking areas, the multimodal center, and UC Davis. The Walk shall incorporate information and educational plaques describing various points of interest along the way. The Discovery Walk shall be designated with signage or symbols placed along the trail. This signage will facilitate the use of the Walk. Descriptive brochures that provide information about the Walk shall be developed and made available at prime locations. The City shall promote public spaces and art along the Discovery Walk. The Walk could also integrate artistic elements that go beyond the inclusion of public art. These elements could include encouraging a public/private partnership for the provision of art, holding a competition to design a repeating identifying pattern on the pavement, use of functional art for amenities and possibilities for temporary (rotating) art on publicly accessible spaces not owned by the City. Pedestrian amenities such as benches, shade structures, drinking fountains, planters, and trash receptacles shall be provided along the way.

- **Third Street Pedestrian/Bikeway**

Third Street between A and B Streets should limit additional through traffic, with the exception of delivery vehicles, and be further enhanced as a bicycle and pedestrian way. This possibility was explored in a plan prepared by Livingston-Blaney in the early 1960's. This would provide a unique connector between UC Davis and the Core Area.

- **Public Plazas**

Public plazas shall be created to provide gathering places in central locations for Core Area users. These focal points tend to pull neighborhoods together, both physically and socially. Plazas shall include pedestrian amenities such as shade trees, landscaping, benches, water features, trellises for shade, and drinking fountains. Plazas shall be places for public art, displays, downtown directories, etc. Plaza sites include, but are not limited to, part of the Brinley lot, the Police Department lot (once abandoned), and the 200 block, on the east side, of G Street.

- **Mid-Block Pedestrian Passageways Incentives**

Within the Downtown Core Area it is desirable to encourage the provision of pedestrian passageways/arcades at mid-block locations to provide access to parking, connections to the Discovery Walk trail system, and connections between streets and alleys.

An incentive program shall be developed that encourages private developers to provide mid-block pedestrian passageways/arcades within their projects. The incentive program shall establish minimum requirements for passageway width, amenities to be incorporated, and appropriate locations.

c. Amenities

Streetscape amenities shall be used for continuity along street frontages and mid-block pathways (Figure 18b). Outdoor furnishings shall be constructed of sound materials, visually attractive, and maintained at high quality. Examples of amenities are:

- **Benches**

Public benches which are designed and constructed of materials that reflect the architectural heritage and ambiance of the Core Area shall be placed throughout the Core Area within the public right-of-way at pedestrian safe crosses/traffic chokes, mid-block crossings, along sidewalks, in plazas, and along the Discovery Walk. Creatively designed benches that function as art as well as a bench would also be appropriate in selected locations. Additionally, private developers are encouraged to incorporate these same designs into their projects.

- **Bollards**

Historically, the bollard was utilized to delineate and separate pedestrian areas from vehicular areas. Its contemporary use in Davis will be as a distinctive furniture element in the Downtown Core. Bollards will be used in areas such as the safe crosses/traffic chokes, mid-block safe crosses and in public plazas. It is recommended that an internally illuminated bollard be used in dimly lit areas along the Discovery Walk. The design of the bollard should be similar in design to the recently installed historic light poles in the Core Area.

- **Bike Racks**

Bike racks shall be of consistent design and placed at key locations throughout the Core Area, including "bike parking lots" in City automobile lots. In addition, they shall be clearly marked. The Public Works Department shall survey the status of bicycle parking in the Core Area. Public Works shall develop a standard plan including policies, design, style, location and number of bicycle racks for the Core Area which is consistent with the Core Area Specific Plan. When this standard plan is finalized, it shall be incorporated by reference into the Core Area Specific Plan.

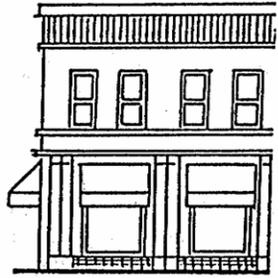
- **Transit stops**

Transit stops shall be placed at convenient locations throughout the Core Area and be clearly marked. In addition, bus shelters, benches, trellises, or other landmarks shall be integrated in their design.

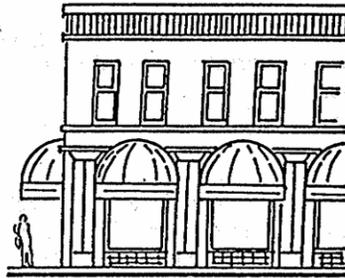
- **Covered Walkways**

Covered walkways shall be integrated with facade or pathway designs to provide protection from the elements as well as vary the street scene appearance.

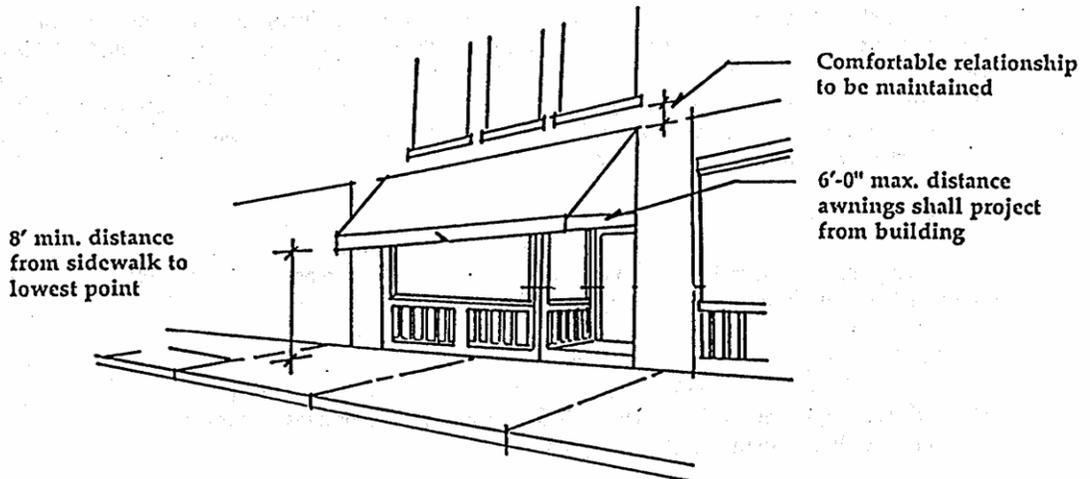
FIGURE 18C
AWNINGS



Awning in scale with facade



Round awning at lower level



Awnings

Awnings can be integrated into facade designs and public elements to provide weather protection and to soften otherwise hard appearances through the use of color and material (Figure 18g). All awnings must meet the requirements of the Uniform Building Code.

Location, size, color and maintenance of awnings are major factors in the overall visual impression of a building. The following guidelines shall be considered:

- Awnings shall be permanently attached to a building;
- Awnings shapes and proportions must be compatible with the character of the building. Awnings shall express the architectural framework, i.e., shape and proportion of the buildings to which they are attached;
- Minimum height of awnings shall be 8'-0" from the lowest point to the sidewalk;
- The highest point of a first-floor awning shall not be higher than the midpoint of the space between the second story window sills and the top of the first floor storefront window, awning, canopy, or transom;
- Internally illuminated awnings should be avoided; and
- Matte finish canvas awnings are preferred over glossy, plastic finish awnings.

- **Garbage Cans and Recycling**

Garbage cans shall remain numerous and in key locations throughout the Core Area. A public space recycling program shall be developed to provide receptacles for the Core Area. All receptacles shall have a sensitive design, special placement consideration, be functional, vandal resistant, and easy to maintain. There will need to be coordination with Davis Waste Removal programs.

- **Newsracks**

Newsracks, like other elements of street furniture, need to be properly designed and placed. A coordinated system of newspaper vending machines shall be established for use throughout the Core Area to reduce the proliferation, clutter, unsightliness and hazards of individually placed newsracks. The system should provide individual spaces for up to eight newspapers within a single cabinet. The cabinet shall be securely anchored to the ground. These newsracks shall not be placed within: three feet of any marked or unmarked street crosswalk; five feet of any driveway; or ten feet of any fire hydrant, traffic signal box, and fire or police callbox. All newsracks shall be painted a consistent color. These consolidated newsracks will replace the individual racks.

- **Drinking Fountains**

Drinking fountains support pedestrian needs and shall be placed at convenient locations throughout the Core Area. Creatively designed drinking fountains that function as art as well as a drinking fountain would be appropriate in selected locations. Drinking fountains must be handicap accessible.

FIGURE 19
STREETLIGHT PLAN

-  EXISTING STREETLIGHT
-  FUTURE STREETLIGHT

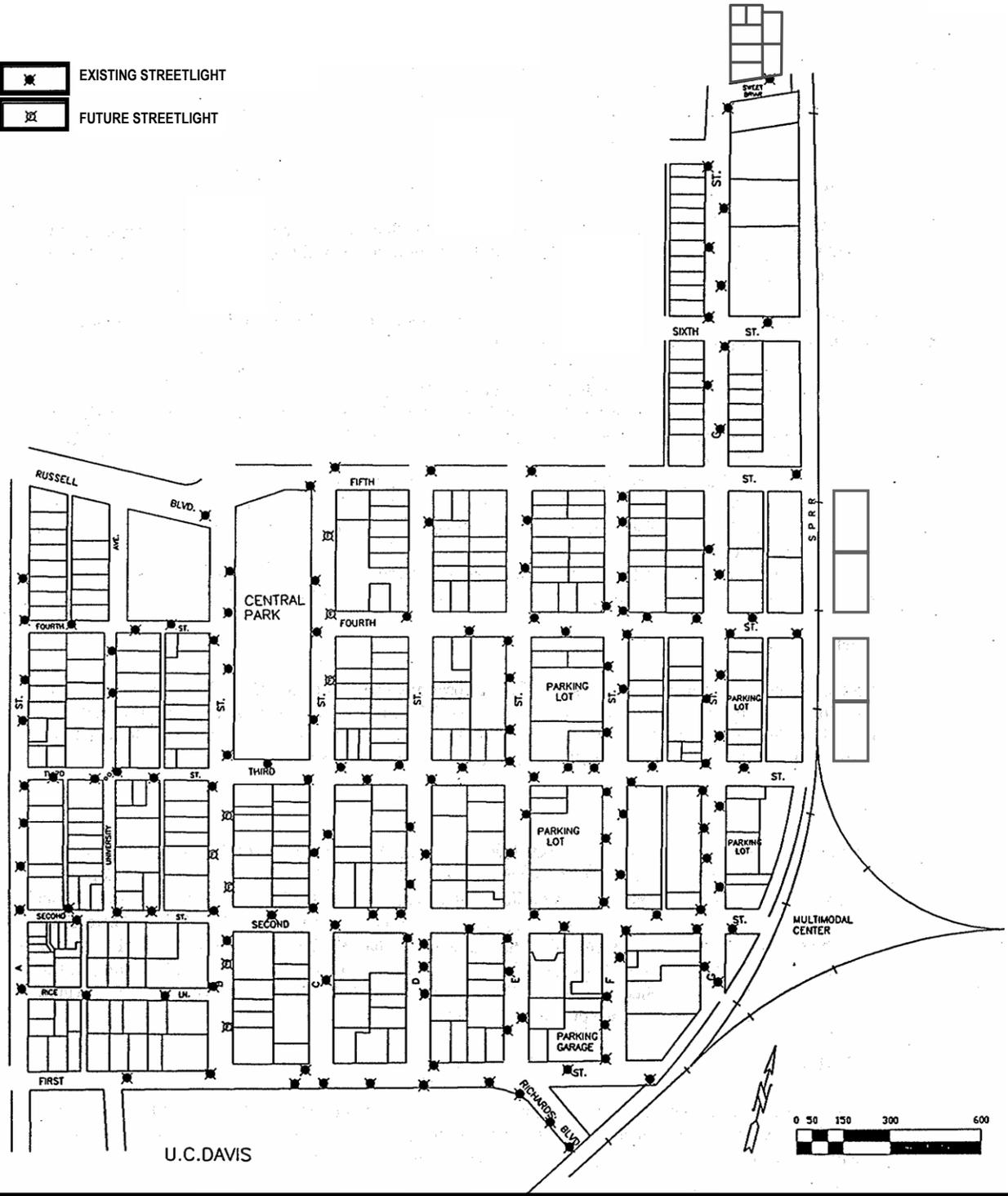
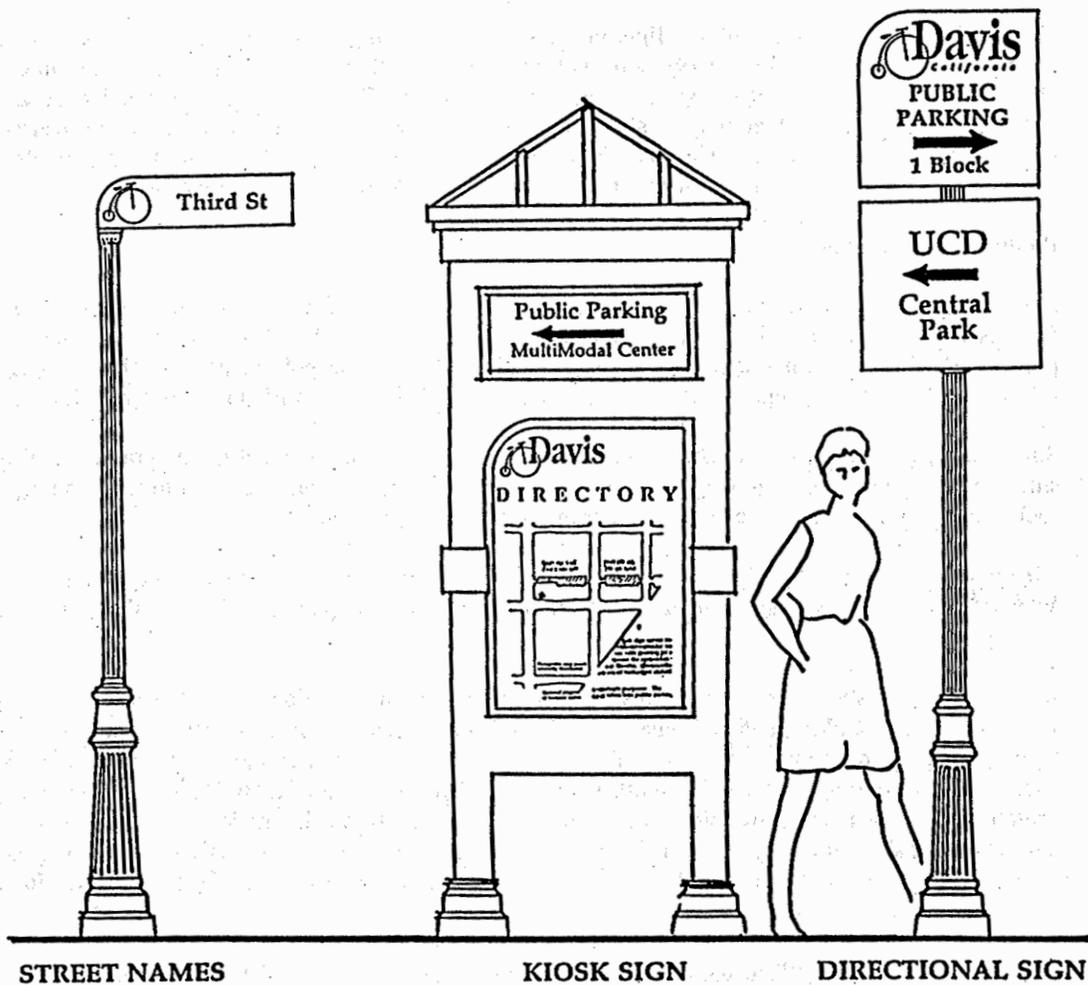


FIGURE 19a

PUBLIC SIGNAGE SYSTEM (TYPICAL)

The Information/Directory kiosk sign serves multiple purposes. In this example, the upper sign panel is meant to direct vehicular travelers into public parking lots and to identify retail uses with parking lot access. The bottom panel is meant to inform and acquaint the pedestrian with all of the retail/office services in the Downtown Core. Illumination of the sign is highly recommended. Hardware on both signs should be painted to match.



- **Public Restrooms and Telephones**

A central facility for restrooms and telephones shall be placed at one or more locations in the Core Area. While facilities exist in Central Park, there is a strong need for another facility clearly marked in the Downtown Core Area. The downtown Police station or the Pence Art Gallery/Amphitheater would be appropriate locations for a public restroom. At these locations the restroom could be easily monitored.

- **Lighting**

While there is a partial downtown lighting plan currently being implemented, it shall be expanded throughout the Core Area (Figure 19). All lighting in plazas, pathways, etc. shall be of consistent design. The use of consistent lighting design can help in identifying the Core Area as a unique entity. It can also aid in differentiating the hierarchy of streets by delineating certain streets as either mainly pedestrian, bicycle, or automobile routes. The historic street lights in the vicinity of Second and G Streets shall remain in the Core Area, either in their existing location or in another Core Area location.

- **Public Signage System**

A unique and specific public signage program shall be developed for the Core Area which identifies features in the Core as well as features outside the Core (for example, UCD, Freeways, Parks). This program shall include three systems of signs including street name pole signs, directional signs, and informative kiosk signs (Figure 19a). Each type is described below, in addition to traffic signal posts:

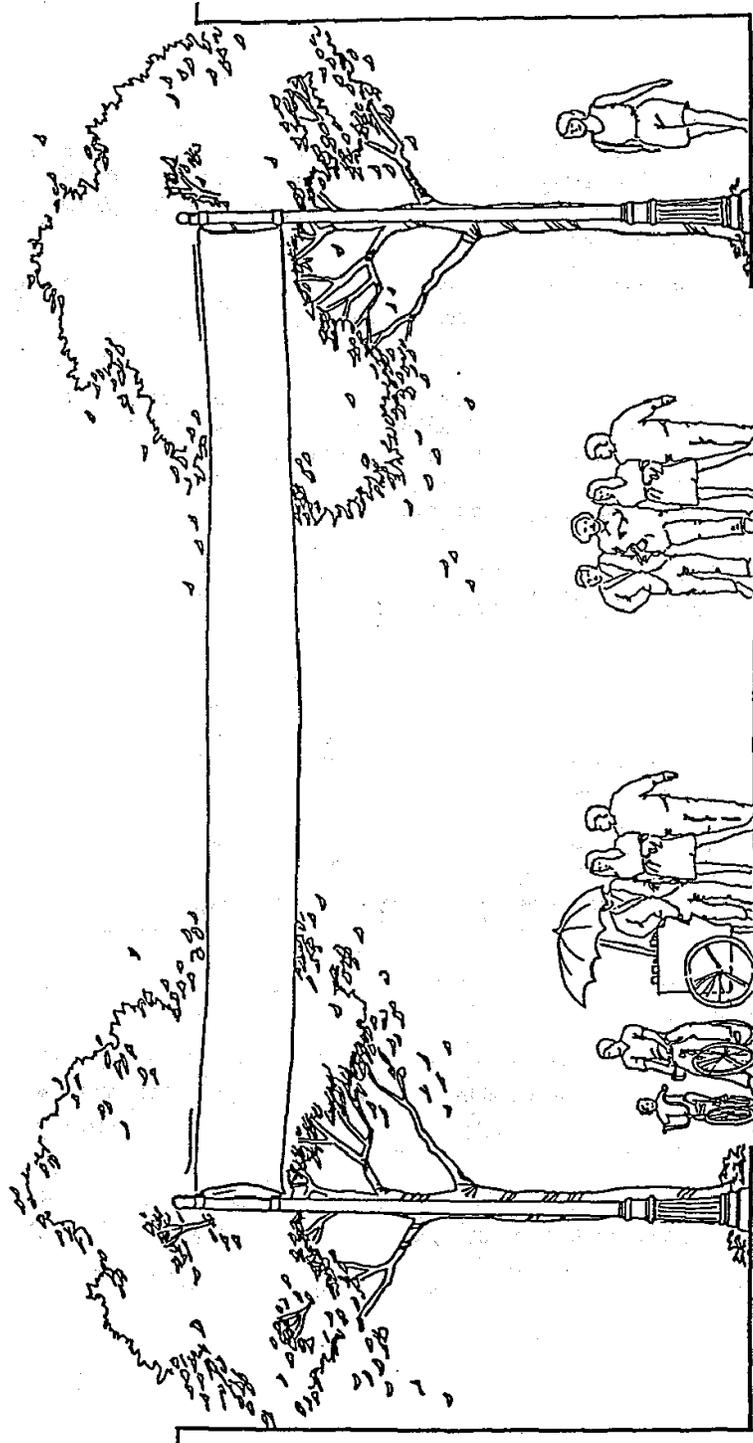
Street Name Pole Signs - The pole would be a historic fluted pole, painted or integral color of black similar to the light pole. Sign height would be between eight to nine feet or similar to City standards for such signs. A minimum of two pole signs per intersection is recommended.

Directional Signs - These signs direct vehicular travelers to features inside and outside the Core. Whenever possible these signs should consolidate information. The pole would match the black historic fluted pole.

Informational Kiosks - These signs provide various types of information. The top sign could direct vehicles to public parking lots and other Core Area features. The lower panel could be a directory and map providing pedestrians with a convenient index. The color of the structural elements of the kiosk should be painted to match the color of other streetscape hardware. The City of Davis logo should appear prominently on the kiosk. To avoid illegal posting of bills, a small warning notice could be included. The City could consider allowing small business advertisements for a fee, which could offset the cost of the sign. Locations of the kiosks could be at selected intersections on two of the four corners (inside area created by safe crosses/traffic chokes) and at major mid-block features.

Traffic Signal Posts - When replacing traffic signals in the Core Area, the Public Works Department shall examine the feasibility of using post top signals to enhance the pedestrian scale of the downtown.

FIGURE 19b
STREET SPANNING BANNERS



- **Banners**

Street spanning banners are to be used in the Core Area to promote local events and to add color to the street scene. Banners that promote local civic events should be placed above the street at First and E Streets and B and Third Streets, as shown on the Specific Plan Urban Design Framework Map (street-spanning banners), Figure 19b.

Seasonal banners should be used on light poles throughout the Core Area to add color and interest. They should be coordinated with specific seasons, holidays, etc. The Davis Downtown Business Association will continue to take the lead in coordinating the banner program for the Core Area.

All banners should be easy to read and be made of durable material. A plain, easy to read lettering style with simple logos is best. Avoid fancy lettering styles like "Old English" and intricate design patterns. The field color should be dark while other lettering and designs should be light. Avoid using white and light pastels as background (field) colors. Support the banners from the top and bottom. Banners should have a matte finish. Avoid shiny plastic finishes. It is recommended that banners should not hang more than (plus/minus) one month.

- **Screening of Backflow Preventers**

Throughout the Core Area there are a number of irrigation system backflow preventers located in landscape areas within the public right-of-way. These are currently screened and secured using a system of heavy cloth bags with locks enclosing the open end. While this system is functional, it is not the most aesthetic. A new screening system shall be developed for backflow preventers (and other similar devices if appropriate) that is capable of contributing to the overall beautification of the street scene while at the same time fulfilling the functional role of screening.

The most appropriate screening system will be one that provides a strong aesthetic and artistic element and that evokes a sense of discovery and surprise. The covers can be fabricated of wood or metal and painted appropriately. They would be designed to slip over the top of the irrigation hardware with a bolt and lock securing it at the bottom. A competition could be held to develop designs for the screening methods.

d. Landscape and Plazas

Landscape elements shall be provided to unify and enhance the downtown environment. Landscaping needs to be viewed as an irreplaceable human resource and treated as such in all project designs and developments. Natural areas shall be preserved in public open spaces. Generous landscaping shall be used throughout the Core Area and shall be incorporated into all projects.

The use of distinct landscaping themes and different types of trees can help to define the hierarchy of streets and to differentiate primary circulation streets from local circulation routes in the Core Area. These elements can help to define those streets that are to be utilized as the main pedestrian, bicycle, or automobile routes. Plazas can be utilized as a primary unifying element in the Core Area. Examples of landscape elements are:

FIGURE 20

PEDESTRIAN SAFETY PLAN



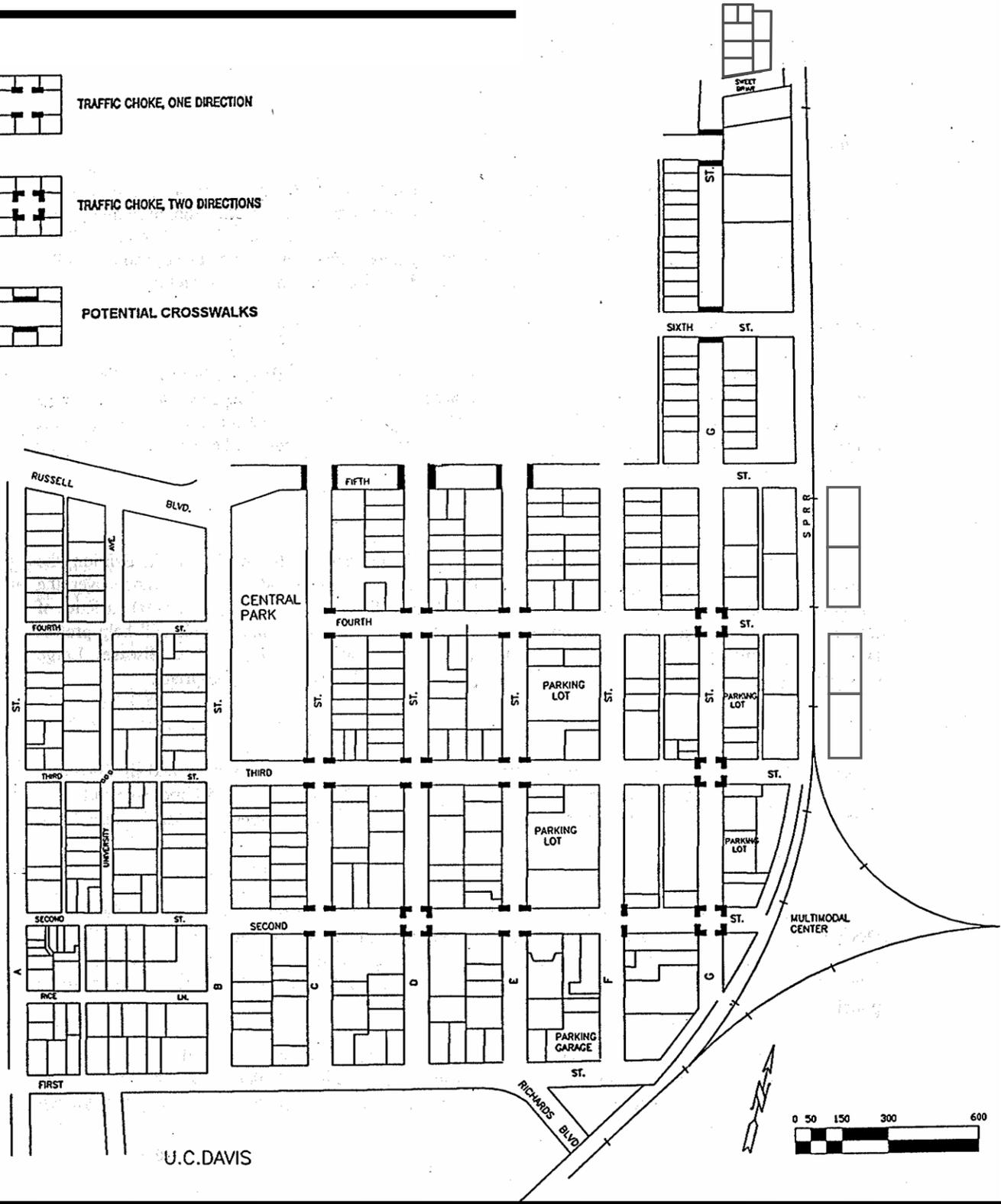
TRAFFIC CHOKE, ONE DIRECTION



TRAFFIC CHOKE, TWO DIRECTIONS



POTENTIAL CROSSWALKS



- **Link to Arboretum**

UC Davis is well known for its arboretum and unique plant collections. A key feature of the arboretum is its tour aspect. Downtown planting should be designed to continue the theme of the arboretum and to add to the pedestrian atmosphere. This link shall continue the natural environment of the arboretum. These raw natural areas can stimulate ideas, emotions, and spiritual awareness, and can connect people with the natural landscape. The City shall work with UC Davis in designing links with the arboretum.

- **Traffic Chokes/Pedestrian Safe Crosses**

Traffic chokes shall be placed at key locations in the Core Area to slow traffic for safety and to discourage automobile use (Figure 20). Traffic chokes lessen street crossing distances for pedestrians and thereby encourage pedestrian activity. Their design shall use the above arboretum concept and shall also provide a location for annual color plantings. Special paving patterns and treatment can be incorporated into the design of traffic choke areas.

- **Street Tree Canopy**

Street tree species and placement should be chosen to give better visibility to storefronts. In addition, the trees shall be of consistent type and variety along blocks and shall provide a shade canopy over the downtown streets at maturity. Landscaping themes shall be utilized which incorporate several varieties of trees along blocks. Long stretches of monoculture plantings shall be discouraged. This will help protect large areas of landscaping from being devastated if certain trees are invaded by pests or disease. Large canopy tree types listed on the City's Master Street Tree List shall be used when appropriate.

- **Landmark Trees and Trees of Significance**

Landmark Trees shall be preserved to the greatest extent possible. Trees of Significance and significant other trees shall be retained whenever possible. Appendices 9 and 10 are the adopted Landmark and Trees of Significance lists --any updates to these lists are incorporated by reference into this Specific Plan.

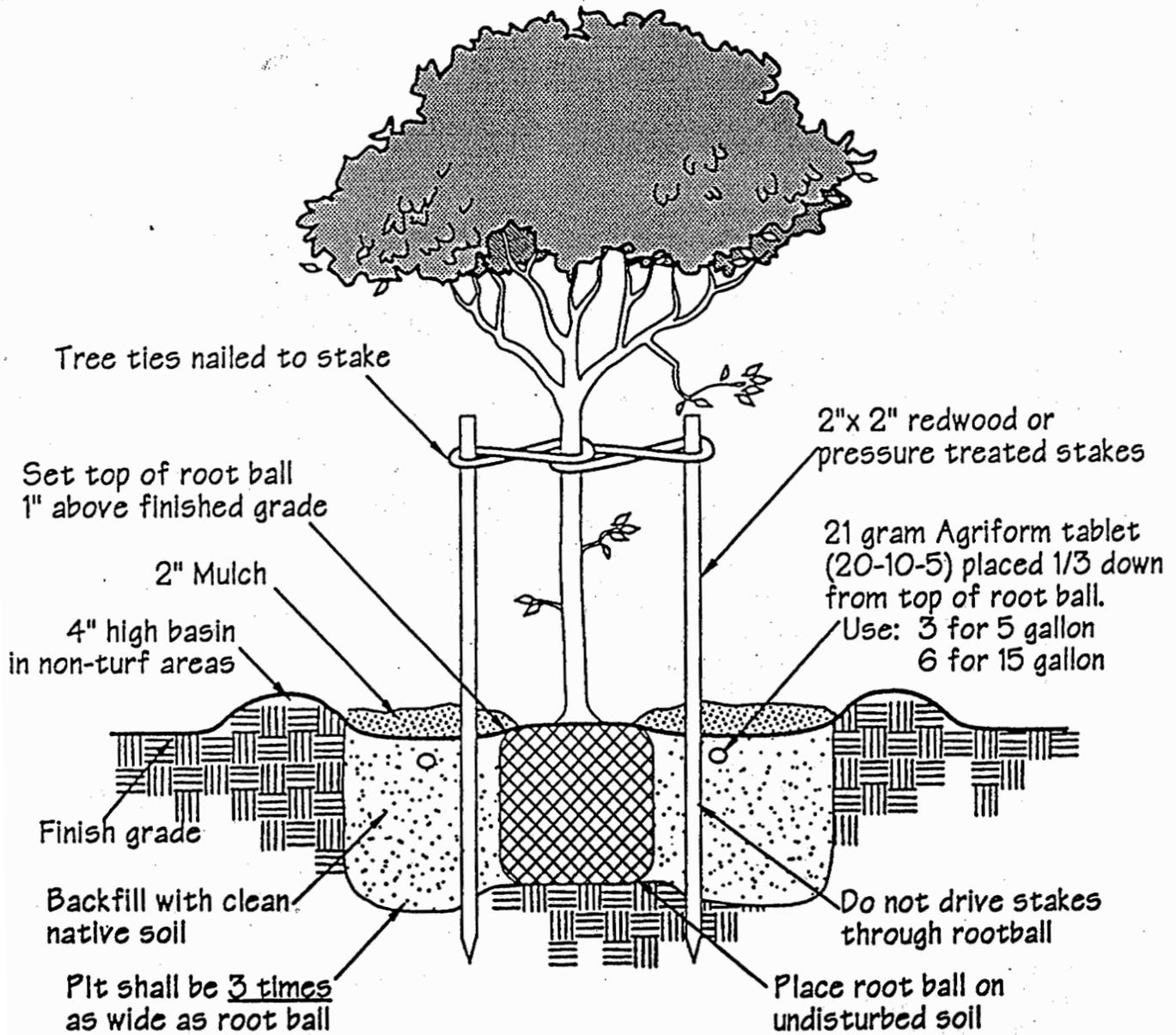
- **Street Tree Replanting Policy**

As the larger mature trees in the Core Area succumb to old age, disease or other natural forces, the City of Davis finds that replacing these community assets to be quite important. In an effort to assure that the Core Area remains fully treed and shady, certain replanting and maintenance policies should be established. In addition to the policies listed above under Street Tree Canopy and Greenstreets, these policies include:

- The City of Davis Standard Details for typical tree planting (Figure 20a) should be used.
- Provide linear planters for trees. The minimum size for linear planters is 4' x 8', with 6' x 8' continuous planting strips preferred. The planters should be enclosed by a bench or curb system whenever possible.
- When funding is available, all replacement specimens should be a minimum 24" box size.

FIGURE 20a

TYPICAL TREE PLANTING



NOTE:

- Stakes to be set east and west, into undisturbed soil, cut just above ties, and placed at least 12" from tree trunk.
- Tree ties made of flexible material, tied just above point where tree bends naturally when held by hand.

FIGURE 20b

TREE BUBBLER DETAILS

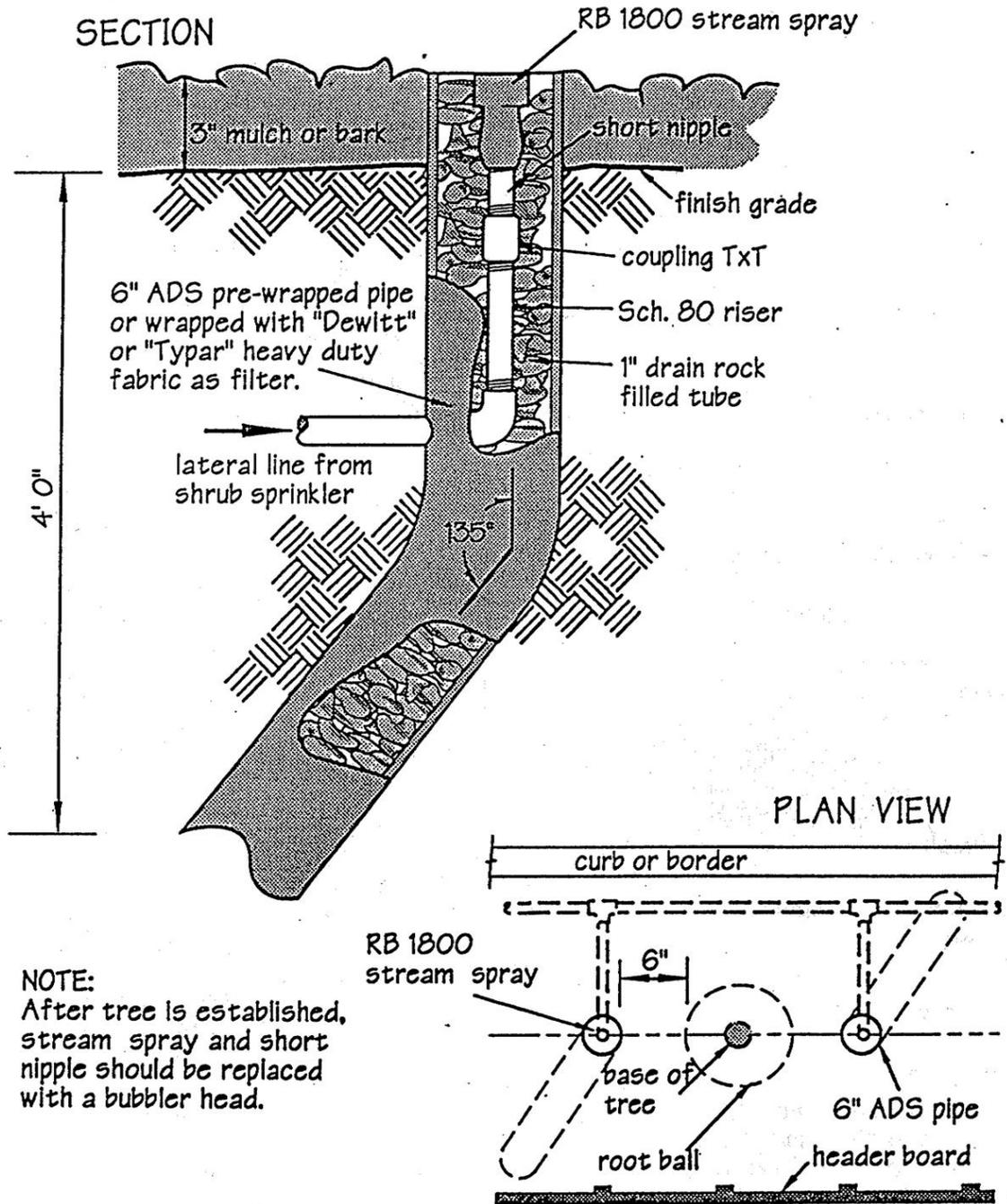
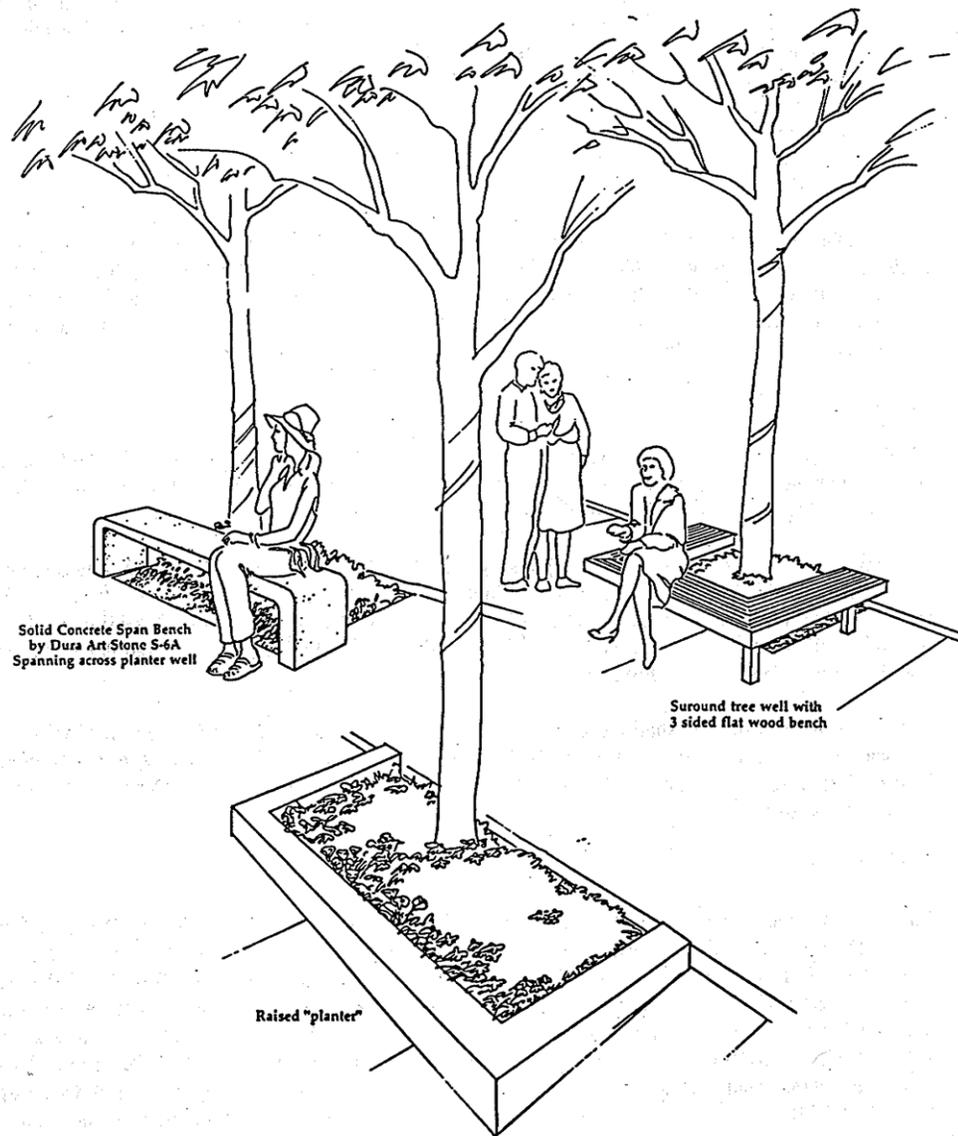


FIGURE 20C
STREET TREE PLANTING AREAS (TYPICAL)



- Mature tree canopies over traffic lanes should be trimmed to a 14 foot height measured from street crown elevation (i.e. from the street crown to the lowest tree limbs).
- To provide clear sight lines to commercial storefronts all mature tree canopies over sidewalks and in front of storefronts, should be trimmed to an 8 foot height from sidewalk elevation or as individually required by individual specie or location.
- Streets exhibiting a monoculture of tree species is discouraged.
- Native plant materials are encouraged.
- Drought tolerant species are encouraged.
- Irrigation to replacement trees is a top priority. See Figure 20b for a tree bubbler detail for deep watering.

A recommended street tree list for the Core Area shall be established which provides selection criteria such as size, color, shape, water requirements, etc. This recommended street tree list and selection criteria shall be developed by the City's Parks and Community Services Department and the Street Tree Commission. Upon approval, this recommended street tree list and selection criteria, shall be incorporated by reference into the Core Area Specific Plan.

- **Street Tree Planting Areas**

Time and experience have pointed out the need for new guidelines for tree replacement in the Core Area. In particular the area dedicated to the planting area at the base of each tree has been too small and left unprotected; the areas experience considerable soil compaction.

The minimum recommended size of the planting area adjacent to the curb is eight feet long by four feet wide, with eight feet long by six feet wide continuous planting strips preferred. Figure 20c shows some of the acceptable means of protecting the base of the tree. Two of the options utilize seating systems while the third utilizes a 6" concrete curb. The planting area should incorporate perennial color flower plantings and irrigation when feasible.

New street tree landscaping shall respect and incorporate any distinctive elements of the existing landscaping. The pattern and plant types shall equal or exceed the quality and intensity of surrounding landscaping. Street tree plantings shall be subject to the review and approval of the Parks and Community Services Department.

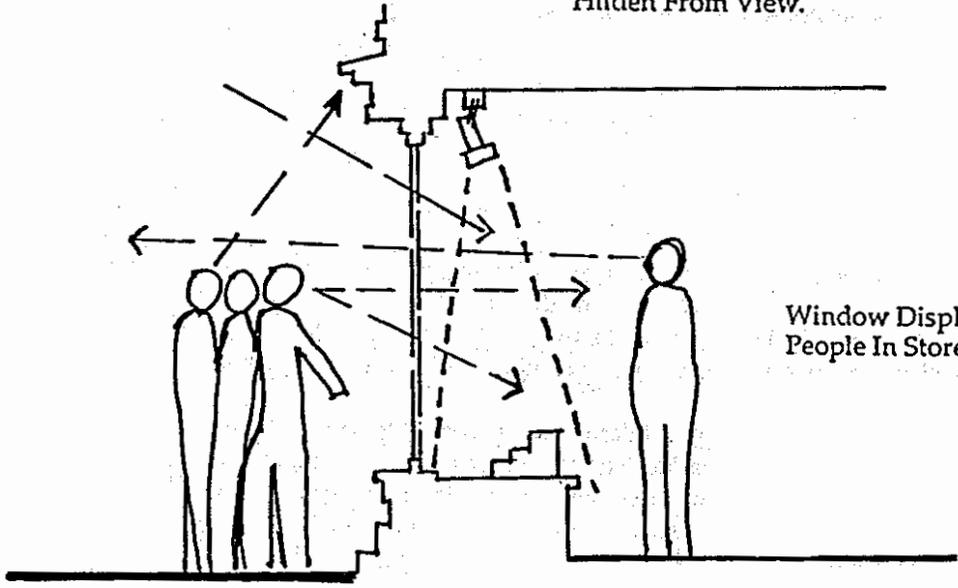
- **Greenstreets**

The Revised Open Space Element (adopted June 27, 1990) of the City's General Plan has designated First, Third, Fifth, A, B and F Streets and Richards Boulevard as Greenstreets. "The primary goal of Greenstreets is to provide convenient and attractive circulation routes for bicyclists and pedestrians, as well as for cars. These circulation corridors shall create a cohesive network that promotes modes of non-motorized transportation by linking activity centers within the City limits." (Revised Open Space Element 2.3.1) "Existing Greenstreets shall be evaluated for adequate tree canopies. Streets found to be lacking sufficient canopy shall be considered for street tree improvement programs." (Revised Open Space Element 3.3 N) Streetscape plans for the Core Area shall give special consideration to the designated Greenstreets and shall incorporate the Greenstreets goals and the guidelines for existing Greenstreets.

FIGURE 21
TRADITIONAL STOREFRONT

Natural Light Should Be Allowed To Penetrate Into The Store Interior.

Light Fixtures Should Be Hidden From View.



Window Display Should Allow People In Store To See Out.

Display Should Allow The Customer A Full View Of The Store Interior.

Window Display Should Be Attractive When Viewed From Both The Sidewalk And The Store Interior.

- **Plazas**

Landscaped plazas provide a variety of usable open space for community use. Plazas can include gardens, courtyards and patios. These spaces can give the neighborhood character, quality, variety, and continuity.

e. Architecture

Architecture in the downtown is diverse, eclectic, and varies in age. This diversity, due to historic evolution, gives the Core Area its unique sense of place. The individuality and heritage in both the residential and commercial areas need to be seen as an opportunity and preserved. Saving older buildings of merit provides a sense of continuity and adds to the complexity and richness of the community. They provide an irreplaceable record of the changing visions and values of the community. They also provide a benchmark by which the community can measure the value of proposed new buildings or additions to existing buildings.

- **Design**

While there should be no restriction on any particular style, generic design themes shall be avoided because they would destroy the uniqueness of the Core Area. Buildings need not mimic the stylistic mannerisms of their neighbors to fit into the area's context. They must, however, share certain fundamental characteristics, functional on-site relationships and common patterns with them.

With this in mind, certain planning and design elements need to be established. Planning elements such as maximum building heights, setbacks, density, open space and historic construction shall be considered. Design elements, such as scale, proportion, texture and visual linkages, shall also be considered. Visual linkages include basic features, spacing between buildings, setbacks, window proportions, entryway placements, landscaping, decorative elements, style, materials, repetition, and silhouette. These all contribute to the unity of a street, neighborhood, or district.

It is critical that projects in the Core Area be designed in context, that is, they shall provide enough visual linkages between existing buildings and the proposed project so as to create a cohesive overall effect. New buildings and/or other projects shall strengthen and enhance the characteristics of their setting, or at least maintain the key unifying patterns. Projects shall respect the positive qualities of the existing environment and the scale and character of nearby structures of merit. Planners shall insure that projects contribute to the overall impression of unity and neighborhood of the Core Area.

Maximum building height shall be reviewed as it applies to the building's density and relationship to adjacent structures. This has typically been restricted to three stories in the Core Area. In the Downtown Core (Retail Stores) area, the first floor of buildings shall be pedestrian oriented. On-site parking shall not be placed in front of buildings along sidewalks; there should be unbroken pedestrian walks and short walking distances between uses. This facilitates window shopping, browsing, people watching and social interaction. While buildings may be constructed to all property lines in some parts of the Core Area, a minimum of 10 percent of the floor area shall be provided as open space. This open space can be

incorporated into ground floor plazas, above ground balconies or other architectural setbacks in the buildings mass. This will provide a more varied and pleasing downtown facade.

Any design guidelines developed for the City shall contain special guidelines for the Core Area which will take into account its uniqueness and architectural heritage

- **Storefront Design Guidelines**

Traditional storefront design with display windows provides for pedestrian interest (Figure 21). Every traditional Davis Downtown facade has a well-defined opening that the original storefront filled. The area is bounded by a pier on either side, the sidewalk on the bottom, and the lower edge of the upper facade on top.

Whether a restoration or more contemporary treatment is being considered, the storefront should be based on a traditional storefront design. The basic configuration can often be derived from old photographs of the building.

The following ideas suggest ways to think about a change in the storefront. Each idea is founded in the design of the traditional storefront; however, these ideas are not historical in nature. They are functional and make the storefront more attractive and accessible to shoppers.

Contain the Storefront - A general rule for future remodeling can be stated as follows: A storefront should be designed to fit inside the original opening and not extend beyond it.

To emphasize this feeling of containment, a storefront might be set back slightly (6 to 12 inches) from the front.

Make It Transparent - The traditional storefront was composed almost entirely of windows, providing maximum light and display. This large glass area creates a visual openness that is part of the overall proportional system of the facade and is as valid today as it was in the past.

For commercial retail storefronts, generally there should be more glass and fewer walls at the storefront level, balanced by more walls and less glass on the upper facade. There shall be no blank walls along pedestrian walkways and windows should not be covered with curtains, etc. Entrances, show windows or other displays which are of interest to pedestrians shall be incorporated into building designs. Clear untinted glass shall be used at ground floor level to allow maximum visual access to the interior of buildings. Mirrored and highly reflective glass shall not be permitted. Historic structures with architectural significance need to be preserved whenever possible. Figure 21a gives an example of a restored or new storefront treatment. To offset some rehabilitation costs, there may be some incentives developed by the Redevelopment Agency to assist in their preservation.

Storefront Materials - The color and texture of the storefront materials should be simple and unobtrusive. The materials used should help create a cohesive overall effect between the project, adjacent and surrounding buildings, and the streetscape. Inappropriate historical themes should be avoided. The Secretary of Interior's Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings shall be used as a reference standard for all designated historical resources.

FIGURE 21a
STOREFRONT TREATMENT

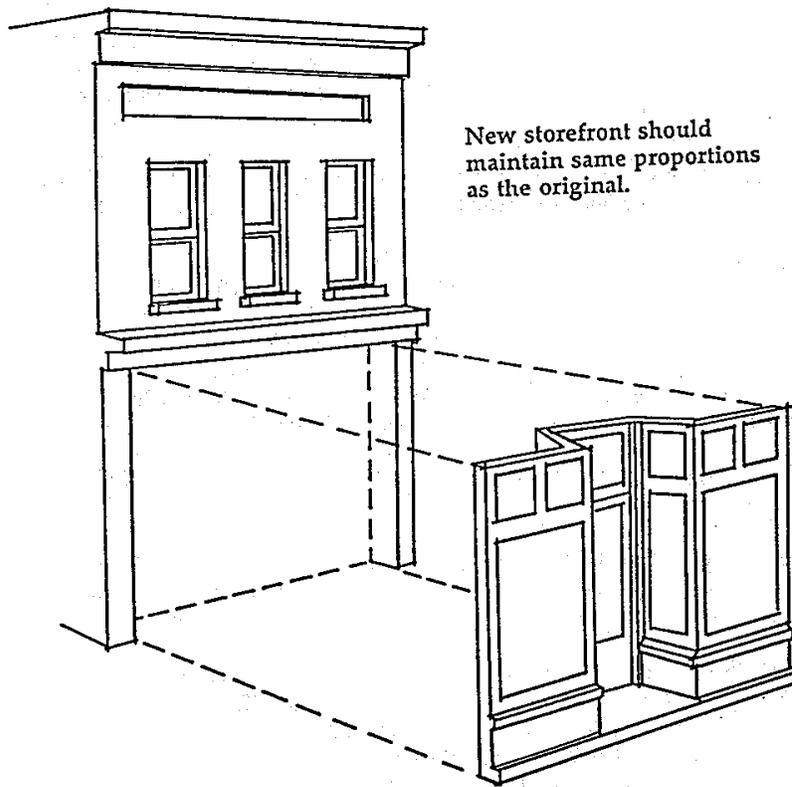
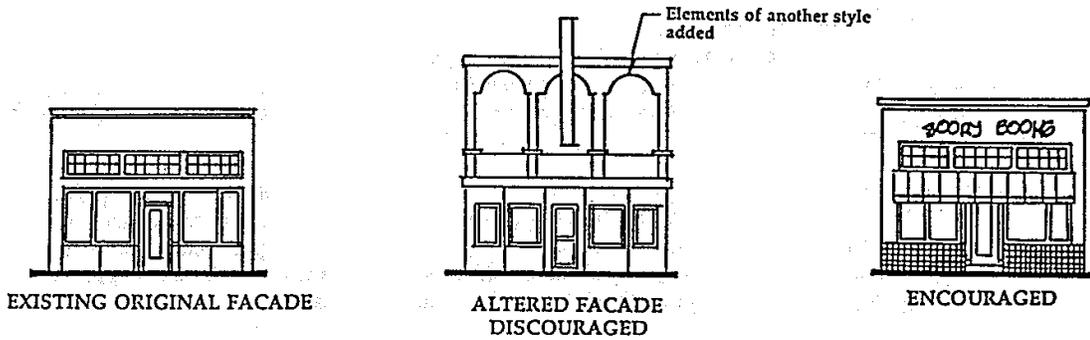
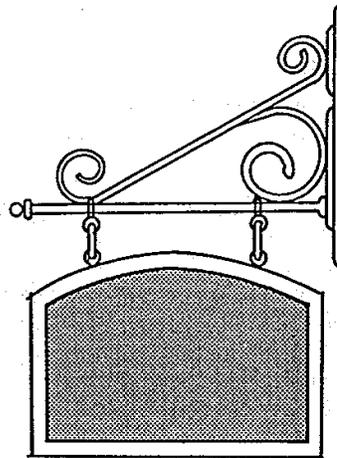


FIGURE 21b
BUILDING DESIGN AND PROJECTING SIGNS



TYPICAL UNDER CANOPY SIGN



Small under canopy signs help reinforce a pedestrian scale.

Keep It Simple

When designing a new storefront or renovating an existing storefront, the emphasis should be on transparency. The basic storefront design should include large pedestrian oriented display windows with thin framing, a recessed entrance, a cornice or a horizontal sign panel at the top of the storefront to separate it from the upper facade and low bulkheads (between 18" -30") at the base to protect the windows and define the entrance.

This basic configuration can be constructed from traditional or contemporary materials, achieving the same result. Figure 21a gives an example of a restored or new storefront treatment. Due to the fact that there is a variety of architectural styles in the Downtown, Figures 21a and 21b (upper detail) are to be used as guidelines that allow for flexibility in individual building design while ensuring that buildings are in scale and that they respect and work with the character of adjacent structures.

- **Storefront Entrance Treatment**

For development in designated Transitional and Mixed Use Districts refer to the Davis Downtown and Traditional Residential Design Guidelines, Mixed Use and Special Character District Sections.

Areas in front of storefronts provide opportunities to create unique pavement entries in two ways. First, the public sidewalk in front of the building can be paved with special materials in keeping with the style and character of the building. The use of combinations of materials is encouraged. Avoid materials that do not stand up well under heavy pedestrian traffic such as flagstone or similar soft materials that may flake/chip easily.

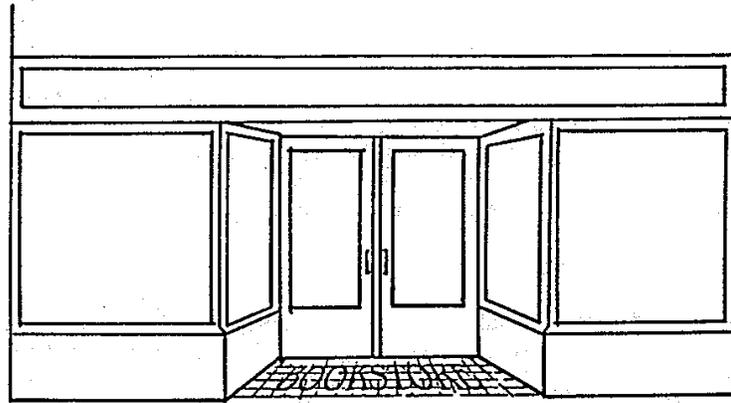
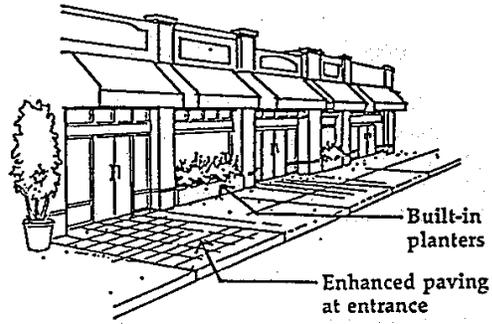
If the storefront has a recessed entry, the area between the front door and the back of the sidewalk can offer an excellent opportunity to personalize the store's entry. Special treatments might include Terrazzo, ceramic tile, or granite pavers with the store's name depicted (Figure 21c).

- **Second and Third Story Guidelines**

Buildings in the Core Area are generally one and two story structures. It is possible that some second and third story additions could be made to certain structures in the Core Area. All second and third story additions shall be of the proper scale and proportion in relation to the existing and surrounding buildings and to the street. The additions shall continue the architectural theme and context of the existing building while being sensitive to the design of adjacent buildings. The additions shall be designed to be integrated into the existing street fabric so that they become integral parts of the neighborhood. In some instances it may be necessary to step back the second or third story addition in order to achieve the appropriate human scale and to create a transition from smaller to taller buildings.

When projects are submitted to the City, the applicant shall provide streetscape elevations and drawings that indicate the height, setbacks, window proportions, decorative elements, style, entryway placement, types of materials, colors, and landscaping to be used not only for the proposed project but also of the adjacent buildings. This is in addition to the other application submittal requirements. This information

FIGURE 21C
STOREFRONT ENTRANCE TREATMENT



will assist the applicant and staff in determining the appropriateness of the proposed project and how it contributes to unifying and enhancing the street and neighborhood.

Second and third story architecture should be governed by these general design guidelines.

- All second and third story facades should be composed of similar materials found on the first floor.
- Window openings should be much smaller than the storefront windows and should remain within the same vertical proportion areas as the storefront windows.
- If awnings are used on upper floor windows, they should be singular for each window and the same color and fabric as the storefront awnings.
- Upper floors should not project out over the storefront facade in most cases.

- **Under Canopy Signs**

The strong pedestrian orientation of the Core Area suggests that small signs suspended from under canopies can help identify business location at the pedestrian (sidewalk) level. The signs are to be included as part of the total square footage of signs allowed.

Pedestrian oriented signs should be unique and creative in their design and impart a sense of "handmade" quality. The signs should be appropriate to the particular business while fitting into the architecture of the building on which they are located (Figure 21b, lower detail).

- **Existing Residential Neighborhoods**

Existing residential neighborhoods and their character shall be protected. They are an integral part of the uniqueness of the downtown. Existing structures with architectural merit shall be retained and reused wherever feasible, even if the uses within the structures are changed. This includes adapting the new use (for example, a commercial instead of residential use) to the old structure.

The design of new and/or rehabilitated buildings shall be sensitive to the character of adjacent and surrounding neighborhoods. Architectural features, such as balconies, patios, stoops and porches, can create usable human spaces, human scale and can help to blend new structures with old. These elements can also be used to create visual interest and to provide casual surveillance of the streets. These features shall be incorporated into new and/or rehabilitation residential designs.

- **Additions to Residential Buildings**

Additions to houses with architectural merit may be necessary to insure their continued use. Modifications such as additions, seismic strengthening, new entrances and exits, and parking facilities should not destroy architecturally significant features, materials, or finishes. Such alterations can fit within the overall scale of the existing house and be compatible with its architectural style.

FIGURE 21d

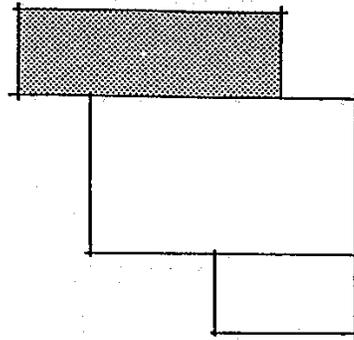
RESIDENTIAL ARCHITECTURAL COMPATIBILITY

DESIRABLE ELEVATION



Roofline addition is integrated into the design of the existing building.

DESIRABLE SITE PLAN



UNDESIRABLE ELEVATION



Flat or shed roofline addition contrasts with the existing roofline.

UNDESIRABLE SITE PLAN

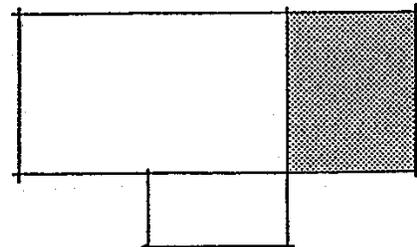
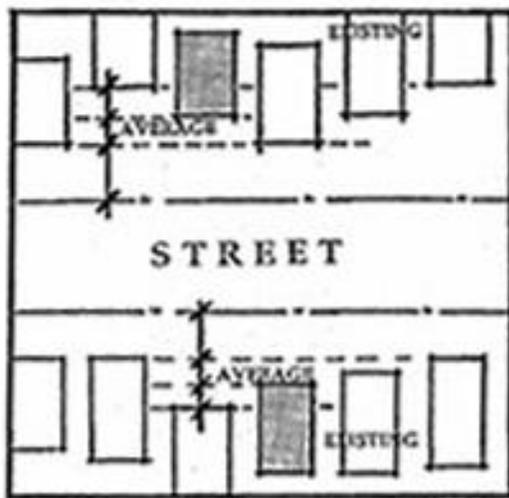
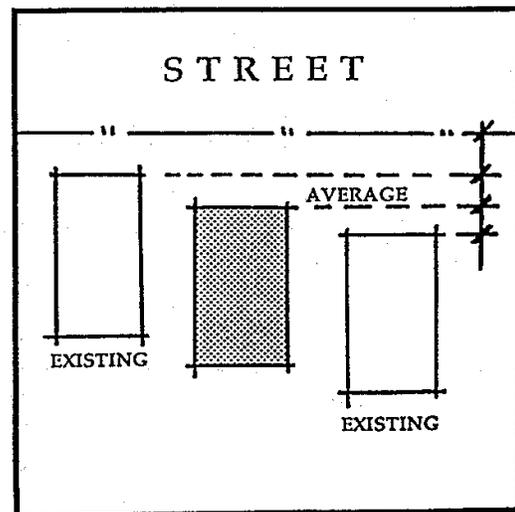


FIGURE 21e

RESIDENTIAL FRONT YARD SETBACKS



Average of setbacks on both sides of street.



Average of setbacks of adjacent buildings.

Site Plan Considerations

Additions should be carefully placed to minimize changes in the appearance of the building from the street. Whenever possible, additions should be placed to the side or rear of the property and should not obstruct the appearance of the building from the street.

Architectural Compatibility

Additions that will alter the facade of the building should first consider alternative means of achieving the same end. For example, skylights can often be used to bring natural light inside rather than installing new windows which may disrupt the facade, or interior seismic bracing can be used rather than exposed exterior bracing which could also obstruct the facade.

Additions to architecturally significant buildings should incorporate the distinctive architectural features of the original building such as:

- window size, shape, and type;
- exterior materials;
- roof style, pitch, material;
- finished floor height;
- color; and
- trim and decoration.

Roof Pitch

The roof of a building, especially its style and pitch, is an important architectural element that must be taken into consideration when planning an addition. Whenever possible the roof style and pitch on the addition should match the original (Figure 21d). Roof materials should also match as close as possible.

Second Story Setbacks

Adding additional stories to an existing residential building will always change a building's proportions and should be carefully designed to follow similar two story examples of the particular style. Integrating the new second story addition into the original design of the building may be easier if the addition is setback from the front facade (Figure 21f).

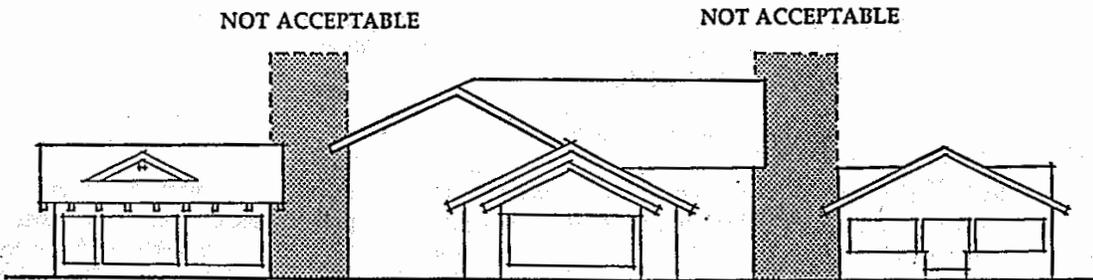
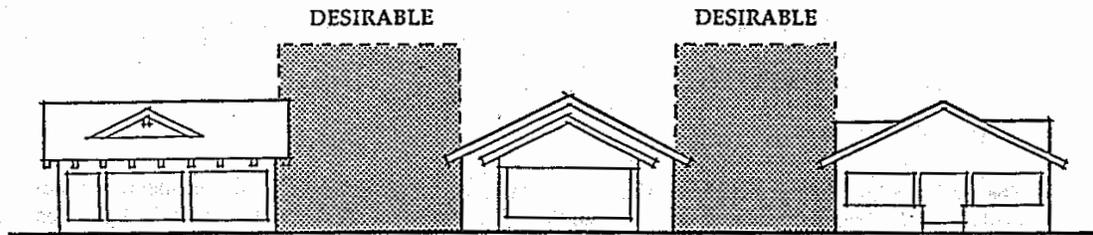
- **New Buildings in Residential Neighborhoods**

The following guidelines are intended to ensure that new infill development complements the pattern and character of existing residential neighborhoods. The single most important issue of infill development is one of compatibility, especially when considering larger developments. When new projects are developed adjacent to older single family residences, concerns exist that the height and bulk of these infill projects do not have a negative impact on smaller scale buildings.

FIGURE 21f

RESIDENTIAL SITE PLANNING CONSIDERATIONS

New single family building sets back upper story where adjacent to one story building to create an appropriate transition.



Site Planning Considerations

New development should continue the functional, on-site relationships of the surrounding neighborhood. For example, common patterns that should be continued are entries facing the street, front porches, and garages/parking located at the rear.

Front yard setbacks for new infill development should follow either of the following criteria:

- Equal to the average setback of all residences on both sides of the street within 100 feet of the property lines of the new project; or
- Equal to the average of the two immediately adjacent buildings.

In cases where averaging between two adjacent existing buildings is chosen, the new building may be averaged in a stepping pattern. This method may work especially well for bungalow-style structures and other styles where it is desirable to provide a front porch along the front facade (Figure 21e).

Moving past a sequence of buildings, one experiences a rhythm of recurrent building masses in relation to spaces between them. This rhythm is necessary in maintaining the element of harmony in a neighborhood's development pattern. New projects should be respectful of the open space patterns created by these setbacks and should provide side yards that respect the existing pattern (Figure 21f, lower detail).

Architectural Considerations

Infill projects should incorporate the distinctive architectural characteristics of development in the surrounding neighborhood, for example: window and door spacing/rhythm, building materials, roof style and pitch, finished-floor height, porches, and the like.

Because infill projects are likely to be taller than one story, their height and bulk can impose on adjacent smaller scale buildings. The height of new projects should be considered within the context of their surroundings. Buildings with greater height should consider setbacks at the second story (Figure 21f, upper detail).

The incorporation of balconies and porches within the building form is encouraged for both practical and aesthetic value. These elements should be integrated to break up large front facades and add human scale.

A variety of materials, when properly used, can reinforce the distinctiveness of the neighborhood. The design of infill projects should incorporate an appropriate mixture of the predominant materials in the area whenever possible.

Color schemes for infill projects should take their cue from the surrounding neighborhood in order to sustain compatibility and harmony.

- **Historic Resources**

The City of Davis is a Certified Local Government which elevates the importance of historic preservation and creates a formal partnership with the State of California for mutual support of preservation efforts. Consideration shall be given to designating the Core Area, as a whole or portions of it, as a Historic District. This will include nominating eligible properties to either the National Register of Historic Places or to the State of California Inventory of Historical Resources, or both. The City of Davis has designated several properties in the Core Area as Historic Resources. The list of these properties is contained in Appendix 12 and a map showing the location of the buildings is contained in Appendix 11 of the Core Area Specific Plan. Any updates to the Historic Resources list, in relation to the Core Area, are incorporated by reference into this Specific Plan.

The Historical Resources Management Commission shall promote public awareness of the prehistoric and historic past of the Core Area by publishing and distributing information about its historic resources and by encouraging educational activities and events.

The Historical Resources Management Commission shall be consulted when decisions are made that affect historical resources in the Core Area. Such decisions might include review of public art, design of street furniture, and public signage.

SECTION V. PLAN ADMINISTRATION AND IMPLEMENTATION

5.1 INTRODUCTION

This Section prescribes the administrative, procedural, and financial mechanisms for the implementation of the Core Area Specific Plan.

5.2 IMPLEMENTATION TASKS

The Core Area Specific Plan is to be administered by the City of Davis Planning and Building Department. The Planning and Building Department shall take the lead in performing the following tasks:

1. Within **one month** following adoption of the Core Area Specific Plan, print sufficient copies of this Specific Plan so that they will be available to the general public at a reasonable cost.
2. Within **one month** after adoption of the Core Area Specific Plan, hold staff training sessions so that all Building and Planning Department employees are aware of new standards in this Specific Plan that take immediate effect.
3. Within **the next budget cycle** after adoption of the Core Area Specific Plan, process amendments to the Major Projects Financing Plan to delete or modify projects that are inconsistent with the Core Area Specific Plan and add projects that are necessary to implement this Specific Plan.
4. Within **one year** after adoption of the Core Area Specific Plan, the Public Works Department shall develop a bicycle parking standard plan which shall include policies, design, style, location and number of bicycle racks for the Core Area which is consistent with this Specific Plan.
5. Within **one year** after adoption of the Core Area Specific Plan, the City shall consider establishing a "Pedestrian Awareness" month to be held during a fair weather time of year, as described in this Specific Plan Circulation Section 3.1,1.F.
6. Within **two years** after adoption of the Core Area Specific Plan, the City, in conjunction with the UC Davis Department of Environmental Horticulture and the U.S.D.A. Forest Service Western Center for Urban Forest Research shall consider a "Downtown Davis Urban Forest Research Demonstration Site" and develop and adopt an "Urban Forestry Master Plan" for the Core Area. This plan shall include a recommended street tree list for the Core Area.
7. Within **two years** after adoption of the Core Area Specific Plan, implementation plans shall be developed for the following projects called for in this plan: Publicly Accessible Art Program (p. 36), the Davis Arch (p. 37), the Discovery Walk (p. 37), and the Image Defining Elements (p. 35). The Civic Arts Commission shall develop the implementation programs for the Publicly Accessible Art and the Davis Arch projects. The Civic Arts Commission, the Historical Resources Management Commission, and the Davis Downtown Business Association shall be consulted as the implementation plans for the Discovery Walk and Image Defining Elements are produced.

CORE AREA SPECIFIC PLAN

8. Within **two years** after adoption of the Core Area Specific Plan, an incentive program shall be considered that encourages private developers to provide mid-block pedestrian passageways within their projects and along designated blocks in which mid-block pedestrian passageways are required. The incentive program shall establish minimum requirements for amenities, based on the guidelines contained in this Specific Plan.
9. As part of on-going code modifications, modify the Zoning Code sections relevant to the Core Area to reflect the concepts in this Specific Plan. These concepts include, but are not limited to:
 - a. Review City parking standards to determine if they reflect actual parking demands and revise parking requirements accordingly. Consideration shall be given to reducing parking requirements if applicants/businesses provide implementable transportation systems management measures.
 - b. If less than 50 percent of ground floor level uses in the Downtown core (Retail Stores) area are devoted to retail uses, consider amending the zoning districts in this area to require retail only at ground floor level.
 - c. Include a definition of "commercial" in relation to Core Area zoning and Core Area Design Combining (C-D) districts to reflect the intent of "retail" use at ground floor level where the word "commercial" is used.
 - d. Amend the zoning districts to require the first floor of buildings in the zoning districts in the Downtown core (Retail Stores) area to be at the same level as the sidewalk, if necessary to accommodate easy pedestrian access.
 - e. Amend the Central Commercial zoning district to have a total floor area no greater than three times the site area (maximum floor area ratio 3:1).
10. All planning applications and building permits for the Core Area shall be reviewed for conformance to the Core Area Specific Plan, and include that analysis in all staff reports on projects in the Core Area. Staff, in their analysis and recommendations made on projects going to hearing before the zoning code amendments discussed above have been processed, shall rely on the Core Area Specific Plan as the basis for their recommendations and shall present their recommendations to decision makers as such.
11. The City's lists of Landmark Trees and Trees of Significance shall be updated periodically for the Core Area based on field surveys by the appropriate City department and/or commission.
12. The City's Street Tree Ordinance and the Landmark Tree and Trees of Significance Lists shall be reviewed by the appropriate City department and/or commission periodically in order to strengthen the powers and abilities of the City to protect both public and private street trees, Landmark Trees and Trees Worth Saving.
13. As part of the work of coming out of the downtown forum, the City shall develop a landscape enhancement and maintenance program for public planter areas in the Core Area. This program shall include the provision of coordinated color flower plantings throughout the year and irrigation when feasible.

CORE AREA SPECIFIC PLAN

14. The City's list of Designated Historical Resources shall be updated by completing the on-going resurvey of the Core Area by the appropriate City department and/or commission.
15. As part of the infill study being prepared for the general plan, City staff shall identify selected buildings in the Downtown Core (Retail Stores) area and identify one story structures that are suited for second and third story additions. Staff shall contact owners to determine their interest in expansion and any possible City/Agency assistance available. Any second and third story additions shall follow the design guidelines outlined in this Specific Plan.
16. After adoption of the Core Area Specific Plan, the City, in conjunction with the Davis Downtown Business Association and the Davis Chamber of Commerce, will continue to work to develop a comprehensive economic development program for the downtown as a whole. This may include the development of a comprehensive database composed of information about land and building space in the downtown. This database may include street addresses with current zoning, general and specific plan designations, current use, parcel and building square footage, and contact persons. This database will need to be kept up to date and should be readily available to the public.
17. An annual Core Area Monitoring Report shall be submitted to the Planning Commission and the City Council discussing private developments, City projects, progress towards completing the long-range components of the Core Area Specific Plan (Discovery Walk, etc.), inconsistencies, outstanding issues, and the need for Specific Plan amendments.
18. Within **three years** of adoption of the Specific Plan Amendments, initiate a process to define elements of a Street Enhancement Program for 3rd Street and B Street rear alleys, including design elements, construction costs, funding mechanisms, cost sharing and implementation schedule.
19. Consider establishment of a Historic Impact Mitigation fee as compensation for demolition of designated historic resources or pre-1945 contributor structures with high integrity. Fees collected would be used for efforts or projects considered to strengthen the historic integrity of the Conservation District as a whole, such as: to facilitate relocation of historic structures to suitable sites; for purchase of historic properties and/or relocation sites; payment of full or partial relocation and rehabilitation costs; restoration or repair of historic resources; and payment for historic research and surveys. The use of historic mitigation fees to reimburse the Redevelopment Agency for the 3rd and J Street site or to contribute to purchase of another site shall be considered.
20. Consider establishment of a Cost Recovery Fee to recoup partial costs resulting from the specific plan amendment process as part of the development costs for development within the B and 3rd Streets Visioning Process Project Area.
21. Consistent with the CASP policies and the Davis Downtown and Traditional Residential Neighborhoods Design Guidelines, the City shall use any in-lieu parking fees collected from new developments in the project area to fund some or all of the following efforts aimed at serving the travel demand of new development sites while minimizing parking on-site or on adjacent neighborhood streets:

CORE AREA SPECIFIC PLAN

- a. Coordinate with UC Davis staff to provide parking on campus for any project developments that will house University functions.
- b. Provide a local car-share program, in conjunction with the University, the Yolo TMA, and other interested agencies, to reduce the need for individual car ownership by project residents and residents of the greater project vicinity.
- c. Consider Redevelopment Agency participation in combination with in-lieu fees to develop a consolidated parking facility.
- d. Consider creating a new Central Park parking district that could provide parking in a series of smaller lots or in a centralized parking structure or lot at a location such as a joint project at the School District site at the north end of the Park should redevelopment be proposed .
- e. Consider reducing parking time limits and the installing parking meters on Third Street to maximize the use of on-street parking for commercial uses.

5.3 PUBLIC IMPROVEMENT PROGRAM

The City's Major Project Financing Plan (MPFP), Capital Improvement Projects (CIP) and fixed assets describe several Core Area public improvement programs. These programs are described below:

Project Number E.6. (CIP 8661) Downtown Plaza

This project is to allow for the design and construction of a landscaped plaza on the block between Second and Third Streets and E and F Streets, known as the E Street Plaza. This plaza will include landscaping, lighting, benches, tables, and art features. In October 1994, City Council authorized \$400,000 in funding to begin development of the E Street Plaza. The target start date for the project is 1995.

Project Number 4E. (fixed asset) Core Area Pedestrian Amenities

This project is to allow for improvements to be made to the Core Area to encourage pedestrian activity. These improvements include benches, bike racks, public art, transit stops, covered walkways, trash and recycling receptacles, public restrooms/telephones, lighting, signs/directories, banners, landscaping, public plazas and traffic chokes. This project has an estimated cost of \$317,300. The source of financing is Redevelopment monies. The target start date for the project is to be budgeted 1995/96 for completion within five years.

Project Number 5E. (CIP 8532) Core Area Street Lighting

This project is to allow for the installation of post-top luminaries on street poles within the area between First and Fourth Streets and C and G Streets. This project has an estimated expenditure to date (1/1995) of \$234,646. The source of financing is Redevelopment monies. The first phase of the project was 1988/89 with completion in 1991/92. An additional \$70,618 is slated for expenditure during 1992/93-1994/95 to fill in dark areas left by the 1991/92 project. Installation and completion is anticipated by June 1995.

5.4 CONCLUSION

One of the guiding principles in the Davis General Plan, and the proposed updated General Plan, is the continued maintenance, enhancement, and promotion of the Core Area as a strong and vital center to the community. The Core Area Specific Plan was created to enhance the vitality of the Core Area while carefully balancing the needs of property owners, City government, business operators, and residents. Implementation of the programs and concepts detailed in the Core Area Specific Plan will help build a downtown that is economically and socially thriving, and will serve Davis residents far into the future.

COREPLN4.NEW

SECTION VI. PLAN PREPARATION

6.1 PLAN PREPARATION

Core Area Specific Plan Task Force

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Cory Parker, Planning Intern
Jason Capitain, Planning Intern
Consultant**

Urban Design Studio (Streetscape Section)

APPENDICES

FIGURE 22
EXISTING ARTERIAL STREETS

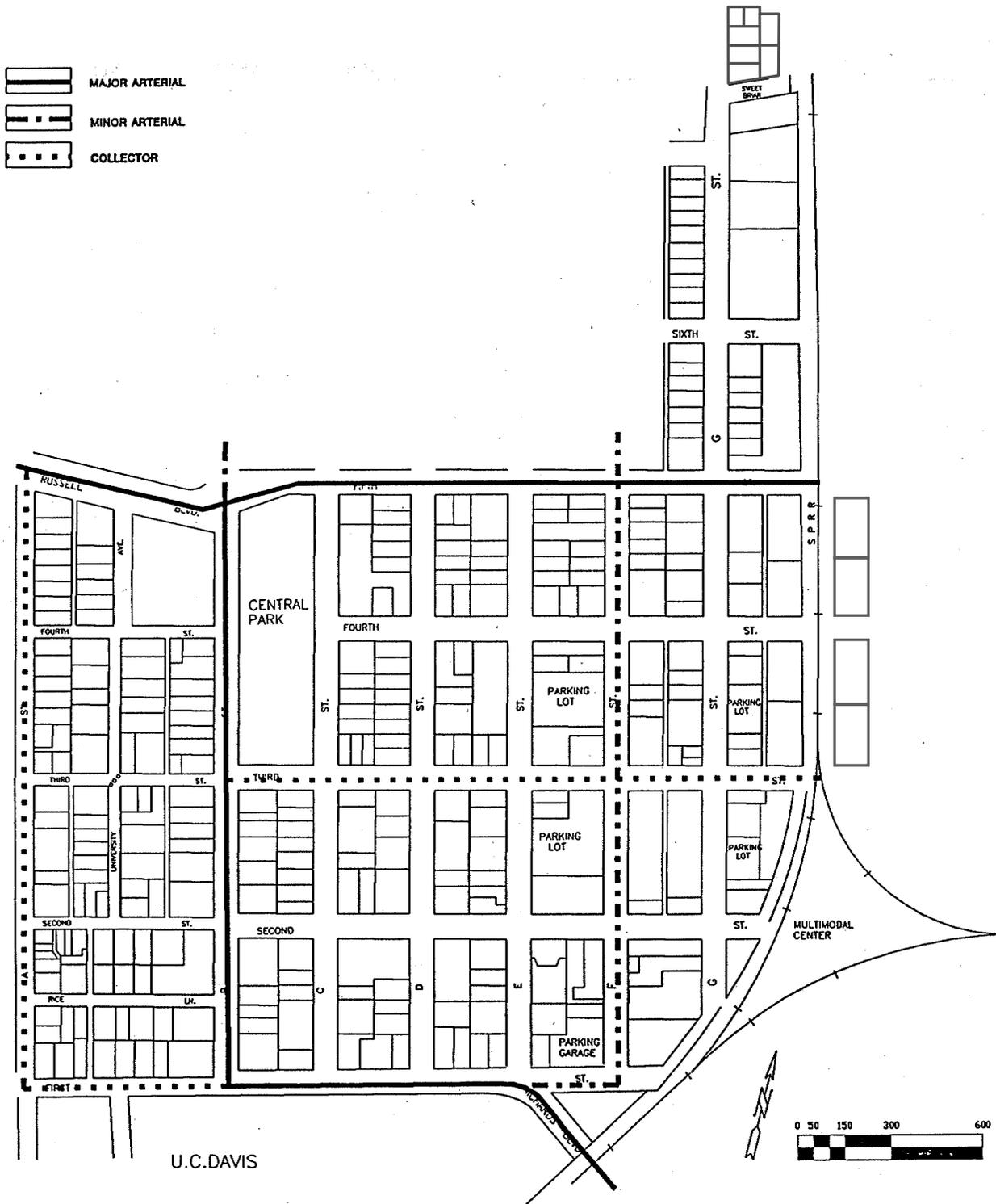


FIGURE 23
EXISTING BIKE LANES AND PATHS

-  BIKE PATH
-  BIKE LANE

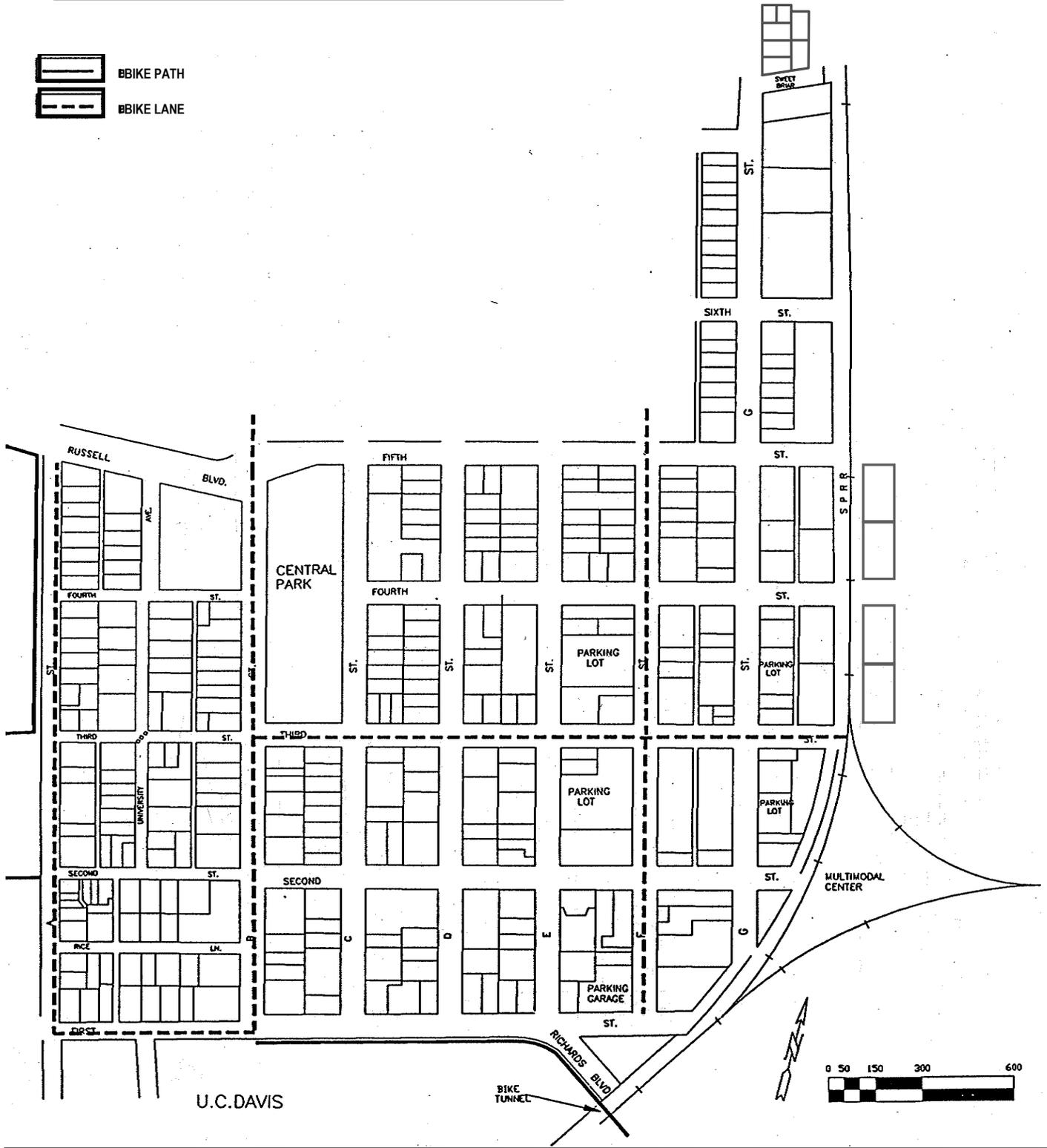


FIGURE 24
EXISTING BUS ROUTES

-  YOLO BUS ROUTE
-  UNITRANS BUS ROUTE
-  GREYHOUND BUS ROUTE

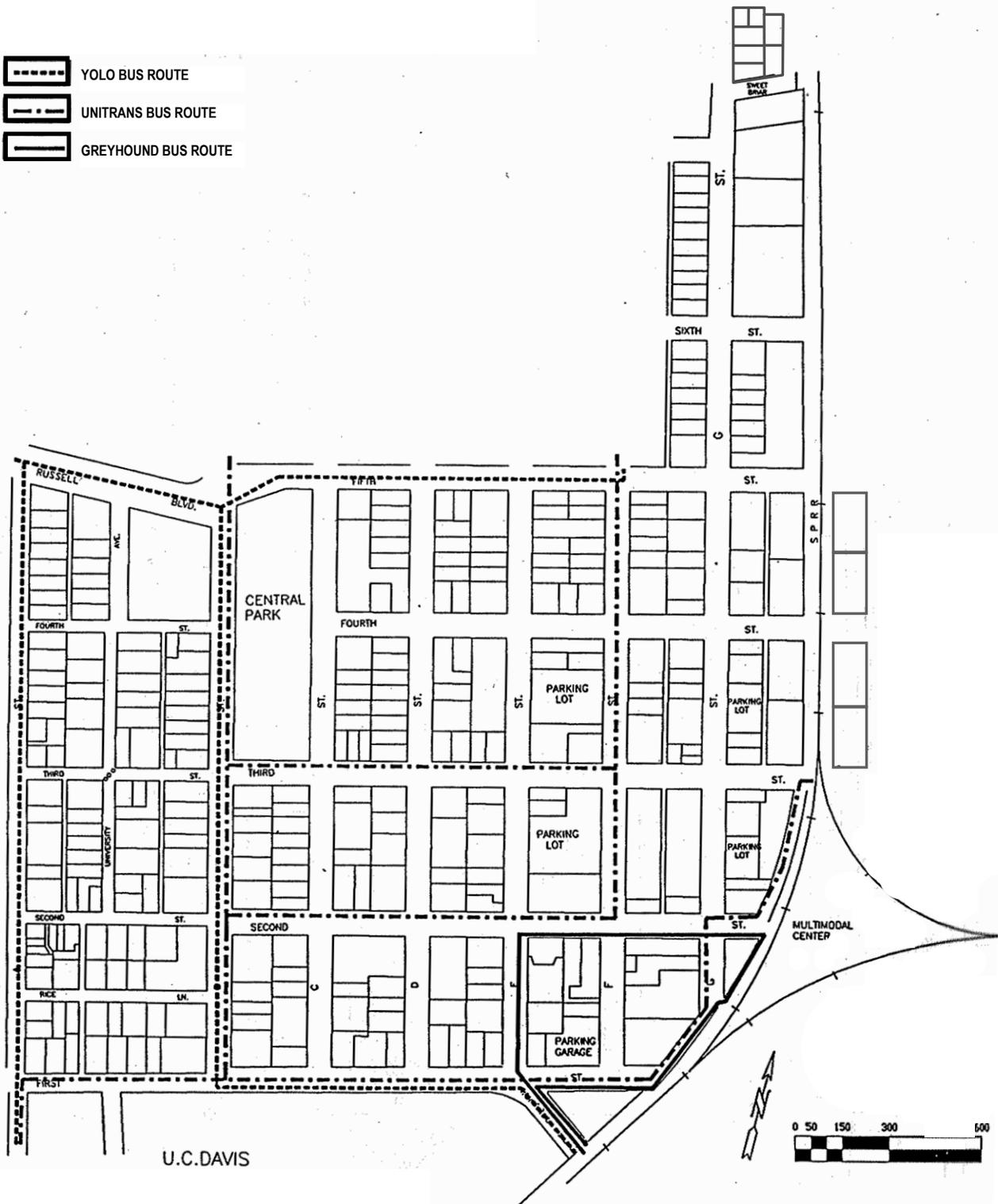


FIGURE 25
EXISTING WATER

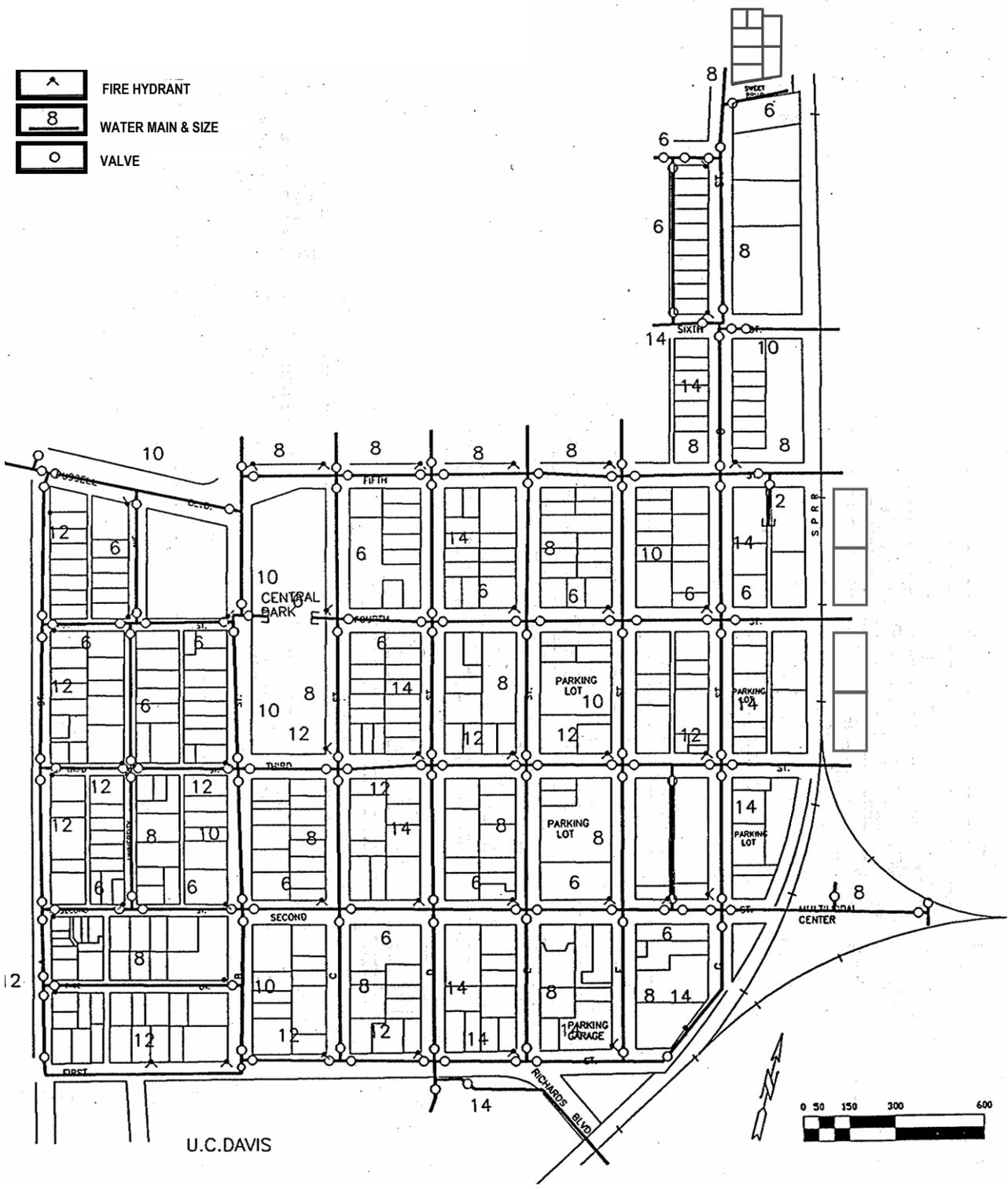


FIGURE 26
EXISTING SEWER

-  SEWER LINE & SIZE
-  SEWER MANHOLE
-  DIRECTION OF FLOW
-  SEWER CLEANOUT

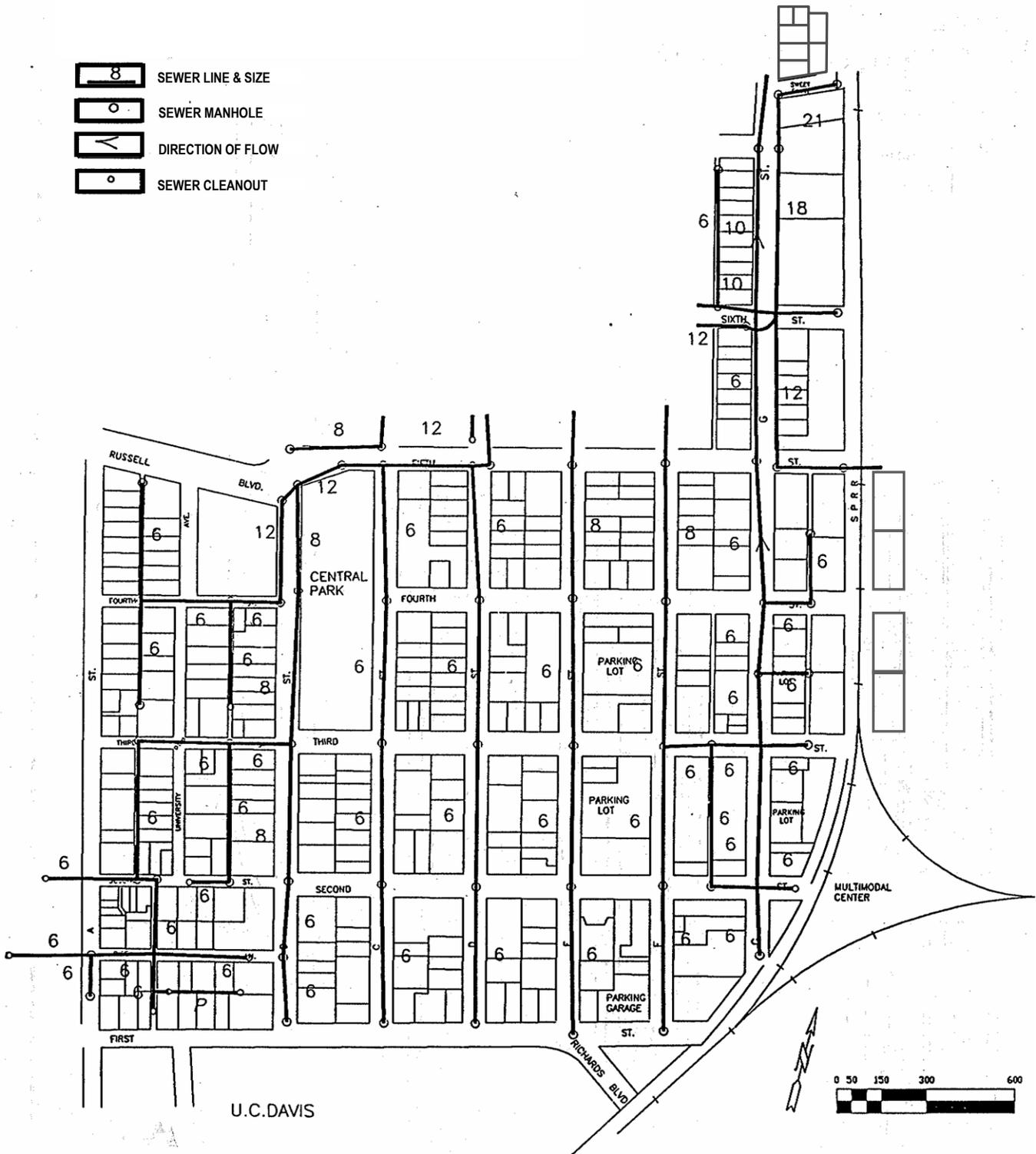


FIGURE 27
EXISTING DRAINAGE

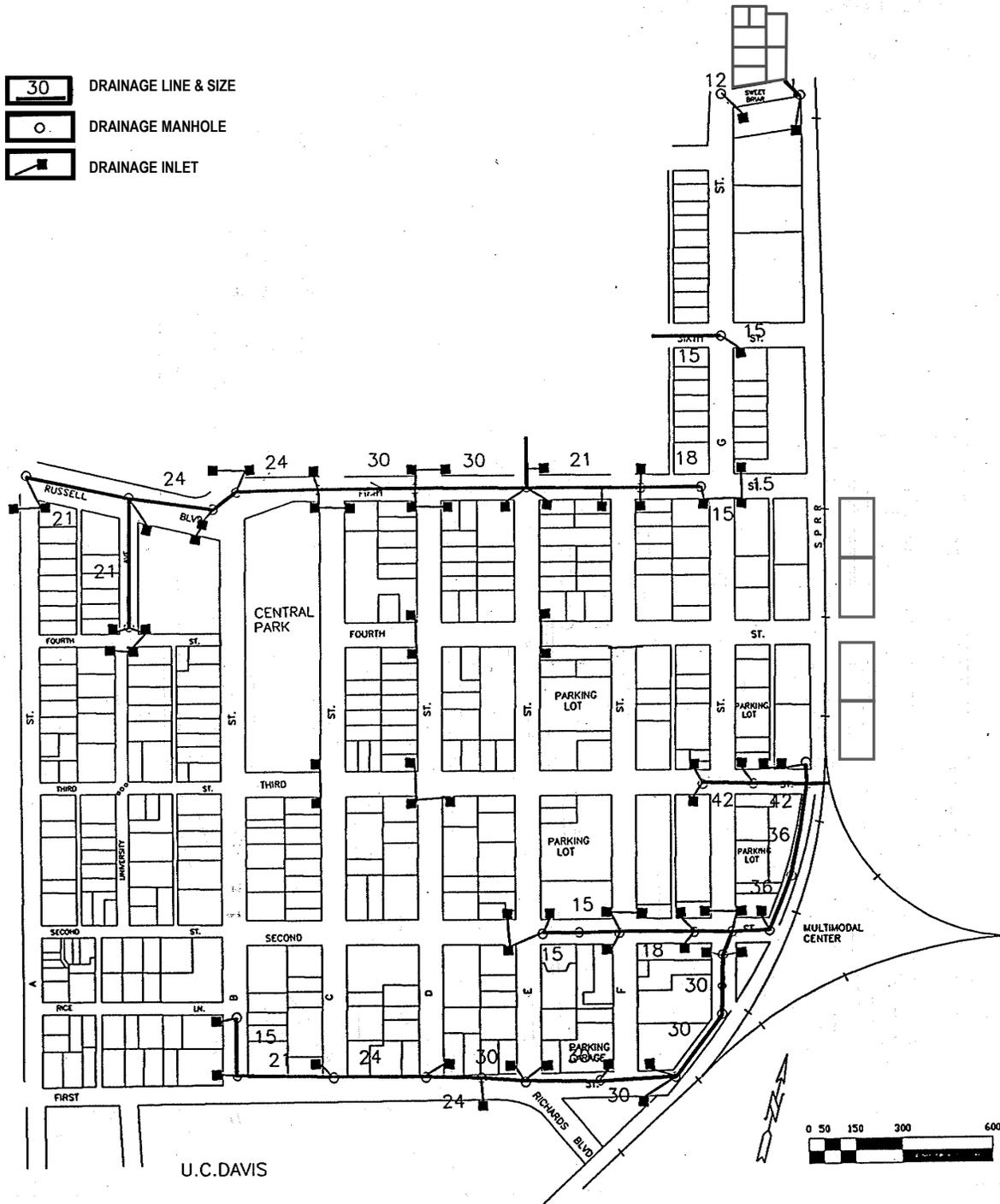


FIGURE 28
EXISTING GAS LINES

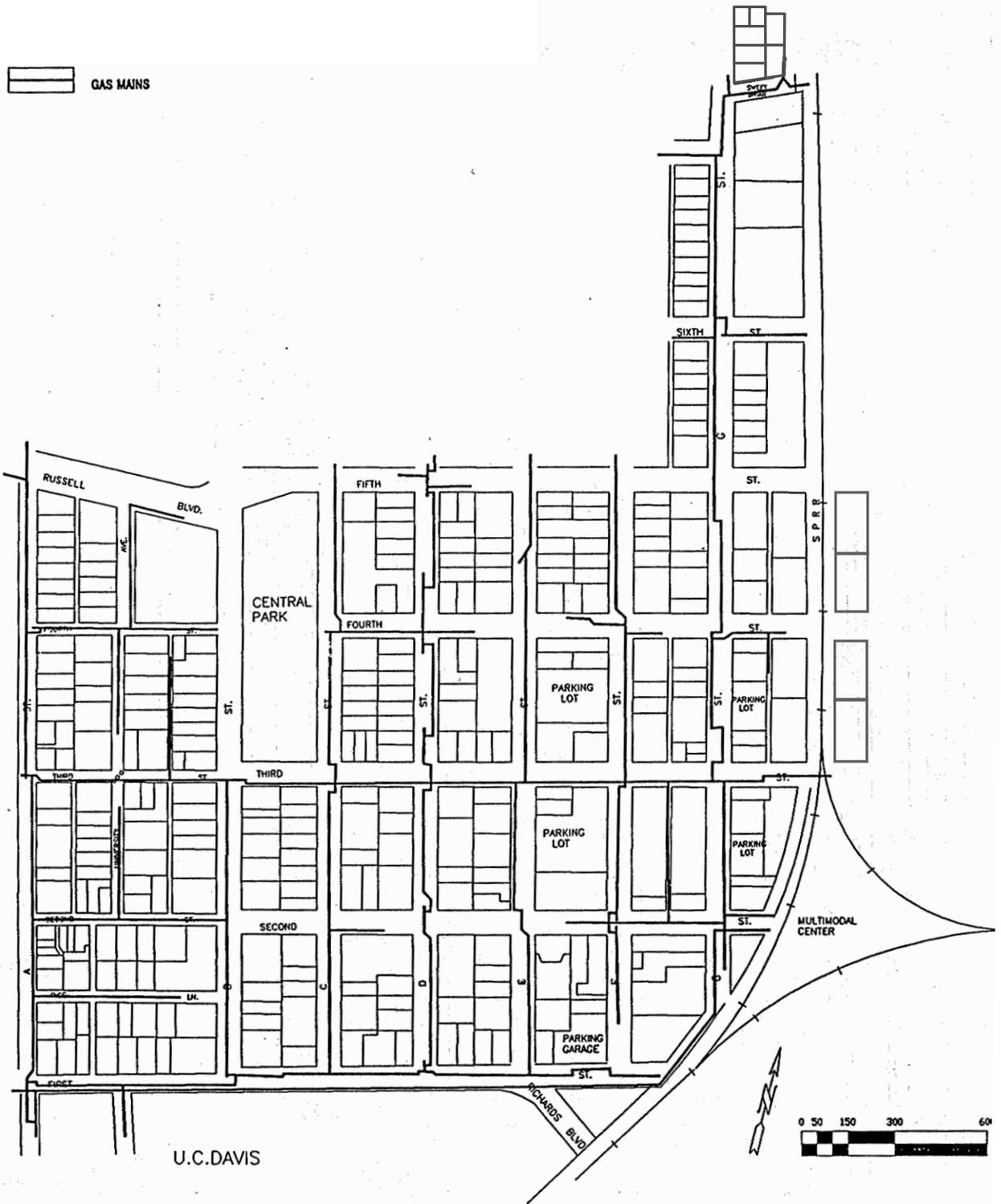
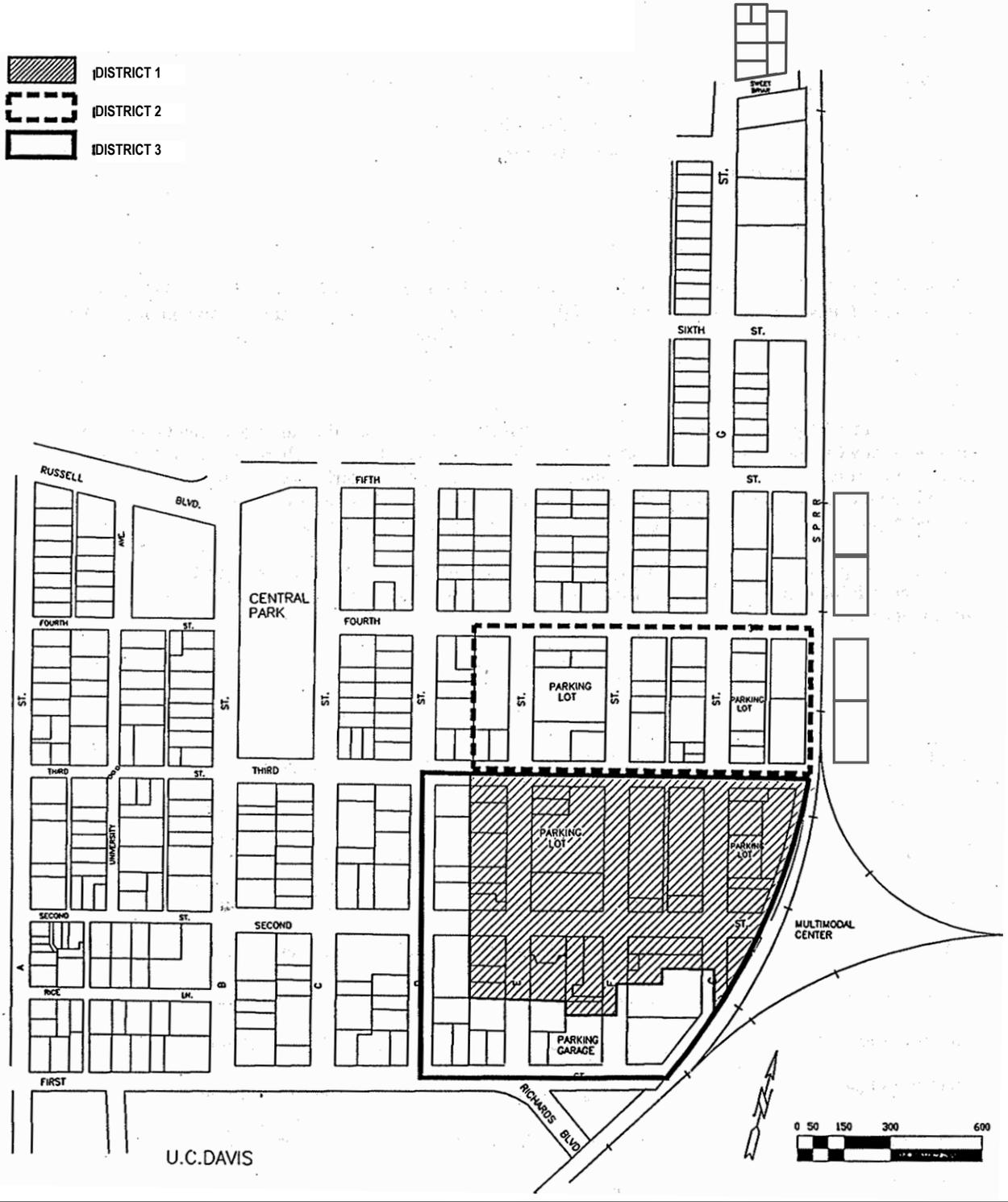


FIGURE 29
PARKING DISTRICTS

-  DISTRICT 1
-  DISTRICT 2
-  DISTRICT 3



LANDMARK TREES IN THE CITY OF DAVIS

Definition:

Landmark Trees may be so designated because they are unique in being:

1. An outstanding specimen of a desirable species
2. One of the largest or oldest trees in Davis
3. Of historical interest
4. Of distinctive form

All such trees would be sound, healthy specimens.

The Street Tree Commission of the City of Davis surveyed all the trees on private as well as public property in the City of Davis in determining the Landmark Trees. City Trees Crews, with the permission of property owners, physically inspected each tree for soundness and health.

Objective:

Landmark Trees help make Davis a unique place in which to live by providing an environmental climate in the hot, windy Central Valley that is a refreshing oasis. To preserve this atmosphere, every effort should be made to preserve these Landmark Trees. Landmark Trees should be memorialized and their noteworthiness made known to the public. Property owners and developers are asked to save these trees and to keep them in good condition for the benefit of present as well as future citizens of Davis.

A second group of trees, Trees Worth Saving, add significantly to the Davis environment but do not meet all the criteria of Landmark Trees. Wherever possible, these trees should be saved and preserved with adequate care.

LANDMARK TREES

(98 total)

<u>Location</u>	<u>No</u>	<u>Scientific Name</u>	<u>Common Name</u>
312 A Street	1	Cedrus atlantica	Mt. Atlas Cedar
736 A Street	1	Quercus lobata	Valley Oak
122 B Street	1	Umbellularia californica	California Bay
749 B Street	1	Juglans regia x hindiii 'Paradox'	Paradox Walnut
230 C Street	1	Ulmus procera	English Elm
16 College Park	1	Pterocarya stenoptera	Chinese Wingnut
16 College Park	1	Koelreuteria paniculata	Golden Raintree
16 College Park	1	Fraxinus velutina 'Modesto'	Modesto Ash
16 College Park	1	Pinus coulteri	Coulter Pine
16 College Park	1	Picea smithiana	Himalayan Spruce
16 College Park	1	Cedrus libani	Cedar of Lebanon

18 College Park	1	<i>Cedrus deodara</i>	Deodar Cedar
20 College Park	1	<i>Cedrus deodara</i>	Deodar Cedar
21 College Park	1	<i>Quercus agrifolia</i>	California Live Oak
24 College Park	1	<i>Ginkgo biloba</i>	Maidenhair Tree (Ginkgo)
24 College Park	1	<i>Pinus halepensis</i>	Aleppo Pine
26 College Park	1	<i>Celtis sinensis</i>	Chinese Hackberry
28 College Park	1	<i>Sequoia sempervirens</i>	Coast Redwood
32 College Park	1	<i>Acer macrophyllum</i>	Big Leaf Maple
35 College Park	1	<i>Cedrus deodara</i>	Deodar Cedar
36 College Park	1	<i>Quercus lobata</i>	Valley Oak
53 College Park	1	<i>Platanus acerifolia</i>	London Planetree
56 College Park	1	<i>Cedrus deodara</i>	Deodar Cedar
64 College Park	1	<i>Celtis sinensis</i>	Chinese Hackberry
212 D Street	1	<i>Quercus lobata</i>	Valley Oak
620 D Street	1	<i>Pinus coulteri</i>	Coulter Pine
630 D Street	1	<i>Cupressus sempervirens</i>	Italian Cypress
102 E Street	1	<i>Cedrus deodara</i>	Deodar Cedar
516 E Street	1	<i>Laurus nobilis</i>	Grecian Laurel
524 E Street	1	<i>Sequoia sempervirens</i>	Coast Redwood
340 F Street	1	<i>Cedrus deodara</i>	Deodar Cedar
417 F Street	1	<i>Libocedrus decurrens</i>	Incense Cedar
428 F Street	1	<i>Quercus lobata</i>	Valley Oak
433 F Street	1	<i>Euonymus japonica</i>	Evergreen Euonymus
437 F Street	1	<i>Olea europaea</i>	European Olive

16 College Park	1	<i>Picea smithiana</i>	Himalayan Spruce
618 F Street	1	<i>Juglans hindsii</i>	California Black Walnut
633 F Street	1	<i>Juglans regia</i>	English Walnut
F Street Parking	1	<i>Platanus acerifolia</i>	London Planetree
Lot—615 G Street	1	<i>Cedrus atlantica</i>	Mt. Atlas Cedar
647 G Street	1	<i>Liriodendron tulipifera</i>	Tuliptree
216 I Street	1	<i>Juglans hindsii</i>	California Black Walnut
320 I Street	2	<i>Euonymus japonica</i>	Evergreen Euonymus
334 I Street	1	<i>Cedrus deodara</i>	Deodar Cedar
409 J Street	1	<i>Cinnamomum camphora</i>	Camphor Tree
221 K Street	1	<i>Aesculus hippocastanum</i>	White Horsechestnut
532 Miller Drive	1	<i>Persea americana</i>	Avocado
612 Miller Drive	1	<i>Comus walteri</i>	Walters Dogwood
701 Miller Drive	1	<i>Albizia julibrissin</i>	Silktree
501 Oak Avenue	1	<i>Quercus lobata</i>	Valley Oak
631 Oak Avenue	1	<i>Metasequoia glyptostroboides</i>	Dawn Redwood
737 Oak Avenue	1	<i>Zelkova serrata</i>	Japanese Zelkova
1020 Olive Drive	1	<i>Quercus suber</i>	Cork Oak
1031 Olive Drive	1	<i>Quercus suber</i>	Cork Oak
1040 Olive Drive	1	<i>Quercus suber</i>	Cork Oak
1041 Olive Drive	1	<i>Quercus suber</i>	CorkOak
1047 Olive Drive	1	<i>Quercus suber</i>	Cork Oak
1123 Olive Drive	2	<i>Quercus suber</i>	Cork Oak
1151 Olive Drive	1	<i>Quercus suber</i>	Cork Oak
1225 Olive Drive	1	<i>Quercus suber</i>	Cork Oak
1233 Olive Drive	1	<i>Quercus suber</i>	Cork Oak
223 Rice Lane	1	<i>Quercus lobata</i>	Valley Oak
228 Rice Lane	1	<i>Celtis sinensis</i>	Chinese Hackberry

237 Rice Lane	1	<i>Sequoia sempervirens</i>	Coast Redwood
217 Russell Blvd.	1	<i>Acer saccharinum</i>	Silver Maple
217 Russell Blvd.	1	<i>Pinus pinea</i>	Italian Stone Pine
433 Russell Blvd.	1	<i>Juglans regia</i> x <i>hindsii</i> 'Paradox'	Paradox Walnut
521 Russell Blvd.	1	<i>Thuja orientalis</i>	Oriental Arborvitae
330 University Ave.	1	<i>Pistacia atlantica</i>	Mt. Atlas Pistacia
505 2nd Street	1	<i>Prunus lyonii</i>	Catalina Cherry
604 2nd Street	1	<i>Pinus sabiniana</i>	Digger Pine
3rd Street & RR 1	1	<i>Juglans hindsii</i>	California Black Walnut
603 4th Street	3	<i>Cupressus sempervirens</i>	Italian Cypress
621 4th Street	1	<i>Celtis australis</i>	European Hackberry
4th and E Streets	1	<i>Ulmus hollandica</i>	Dutch Elm
4th and C Streets Community Church	1	<i>Sequoia sempervirens</i>	Coast Redwood
5th and D Streets	1	<i>Quercus lobata</i>	Valley Oak
(Junior High School)	2	<i>Pinus halepensis</i>	Aleppo Pine
“ “ “	1	<i>Casuarina cunninghamiana</i>	Cunningham Beefwood
“ ’ “ “	1	<i>Thuja orientalis</i>	Arborvitae
“ ’ “ “	1	<i>Pinus canariensis</i>	Canary Island Pine
“ ’ “ “	1	<i>Cedrus deodara</i>	Deodar Cedar
405 7th Street	1	<i>Acer buergerianum</i>	Trident Maple
235 E. 7th Street	1	<i>Juglans regia</i>	English Walnut
521 E. 7th Street	1	<i>Juniperus virginiana</i>	Red-Cedar Juniper
511 E. 7th Street	1	<i>Pinus halepensis</i>	Aleppo Pine
623 E. 7th Street on E	2	<i>Pinus roxburghii</i>	Chirr Pine
426 E. 8th Street on D	1	<i>Quercus lobata</i>	Valley Oak
512 E. 9th Street	1	<i>Casuarina cummingharniana</i>	Cunningham Beefwood
639 E. 9th Street on F	1	<i>Ulmus parvifolia</i>	Chinese Elm

620 E. 10th Street	1	<i>Ailanthus altissima</i>	Tree-of-Heaven
616 E. 10th Street	2	<i>Ailanthus altissima</i>	Tree-of-Heaven

Revised Date: September 25, 1991

Landmark.tre

TREES WORTH SAVING IN THE CORE AREA
1st to 5th Street and A to L Streets

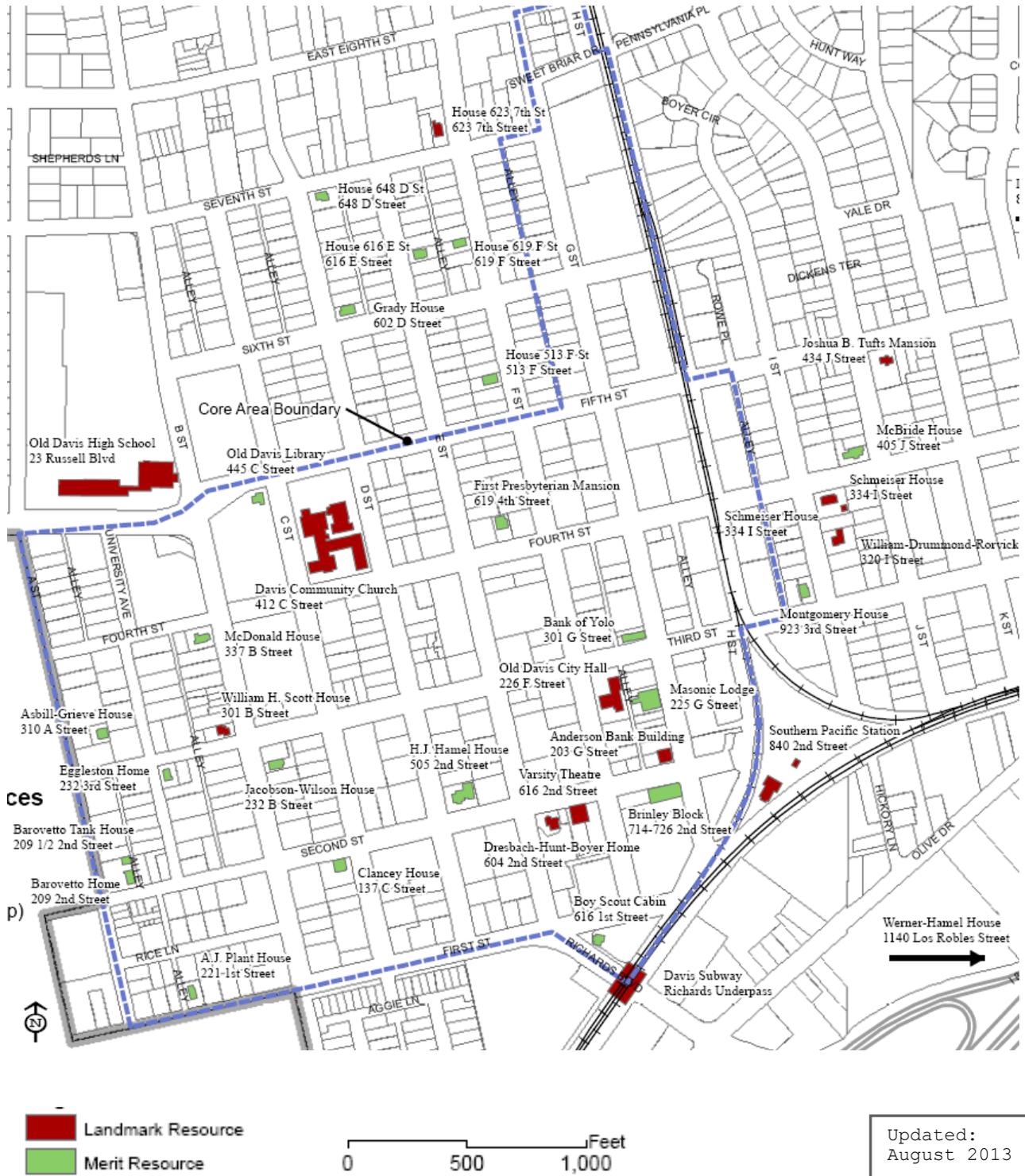
LOCATION	COMMON NAME	SCIENTIFIC NAME
241 First Street	3 London Planetree	Platanus acerifolia
241 First Street	Chinese Hackberry	Celtis sinensis
245 First Street	2 London Planetree	Platanus acerifolia
217 Second Street	Dutch Elm	Ulmus hollandica
232 Second Street	Chinese Hackberry	Celtis sinensis
234 Second Street	2 Chinese Hackberry	Celtis sinensis
222 Second Street	Valley Oak	Quercus lobata
240 Second Street	Cork Oak	Quercus suber
401 Second Street	European White	Birch Betula verrucosa
505 Second Street	Coast Redwood	Sequoia sempervirens
215 Rice Lane	English Hawthorn	Crataegus oxyacantha
233 Rice Lane	Catalina Cherry	Prunus lyonii
233 Rice Lane	Chinese Hackberry	Celtis sinensis
242 Rice Lane	Maidenhair tree	Ginkgo biloba
246 Rice Lane	2 London Planetree	Platanus acerifolia
232 3rd Street	Paradox Walnut	Juglans regia x hindsii
240 3rd Street	Incense Cedar	Calocedrus decurrens
AGR 3rd Street	Deodar Cedar	Cedrus deodara
1221 3rd Street	Almond	Prunus amygdalus
4th and C Streets	Italian Cypress	Cupressus sempervirens
503 4th Street	Valley Oak	Quercus lobata
509 4th Street	Evergreen Euonymus (shrub)	Euonymus japonica
621 4th Street	London Planetree	Platanus acerifolia
" " "	Deodar Cedar	Cedrus deodara
" " "	Crepe Myrtle	Lagerstroemia indices
" " "	Coast Redwood	Sequoia semoervirens
603 4th Street	Chinese Pistache	Pistacia chinensis
" " "	Loquat	Eriobotrya japonica
" " "	Deodar Cedar	Cedrus deodara
619 4th Street	Japanese Maple	Acer almatum
5th and D Streets	3 Chinese Wingnut	Pterocarya stenoptera
1205 5th Street	English Walnut	Juglans regia
1107 5th Street	Paradox Walnut	Juglans regia x hindsii
116 A Street	2 Almonds	Prunus amygdalus
201 A Street	Almond	Prunus amygdalus
214 A Street	Almond	Prunus amygdalus
302 A Street	Deodar Cedar	Cedrus deodara
318 A Street	Chinese Hackberry	Celtis sinensis
322 A Street	Smoothleaved Elm	Ulmus carpinifolia
203 3rd on A Street	Blackwood Acacia	Acacia melanoxylon
340 A Street	Flowering Ash	Fraxinus ornus
406 A Street	Black Locust	Robinia pseudoacacia
412 A Street	Douglas Fir	Pseudotsuga menziesii
426 A Street	Silver Maple	Acer saccharinum
430 A Street	Valley Oak	Quercus lobata
" " "	Coast Redwood	Sequoia semoervirens
136 A Street	2 Chinese Hackberry	Celtis sinensis
" " "	3 Almond	Prunus amygdalus
" " "	Deodar Cedar	Cedrus deodara
440 A Street	Western Sycamore	Platanus racemosa
" " "	Valley Oak	Quercus lobata
212 University Avenue	Blackwood Acacia	Acacia melanoxylon

LOCATION	COMMON NAME	SCIENTIFIC NAME
227 University Avenue	Paradox Walnut	Juglans regia x hindsii
215 University Avenue	London Planetree	Platanus acerifolia
219 University Avenue	Paradox Walnut	Juglans regis x hindsii
239 University Avenue	American arborvitae	Thuja occidentalis
330 University Avenue	Tulip tree	Liriodendron tulipifera
334 University Avenue	Silver Maple	Acer saccharinum
340 University Avenue	Bigleaf Maple	Acer macrophyllum
116 B Street	Smoothleaved Elm	Ulmus carpinifolia
123 B Street	European White Birch	Betula verrucosa
" " "	2 Incense Cedar	Calocedrus decurrens
229 B Street	Chinese Hackberry	Celtis sinensis
241 B Street	Coast Redwood	Sequoia sempervirens
" " "	Deodar Cedar	Cedrus deodara
257 2nd Street	3 Black Locust	Robinia pseudoacacia
305 B Street	English Hawthorn	Crataegus oxycantha
315 B Street	Chile Mayten	Maytenus boaria
236 B Street	Strawberry Madrone	Arbutus unedo
137 C Street	Deodar Cedar	Cedrus deodara
4th and C Streets	Deodar Cedar	Cedrus deodara
" " " "	Coast Redwood	Sequoia sempervirens
C Street, 3rd and 4th	Valley Oak	Quercus lobata
" " " "	Siberian Elm	Ulmus pumila
322 C Street	Incense Cedar	Calocedrus decurrens
310 C Street	Tobira Pittosporum	Pittosporum tobira
" " "	California Live Oak	Quercus agrifolia
209 C Street	Digger Pine	Pinus sabiniana
327 C Street	Catalina Cherry	Prunus lyonii
" " "	Valley Oak	Quercus lobata
4th and C Streets	Incense Cedar	Calocedrus decurrens
224 D Street	English Hawthorn	Crataegus oxycantha
" " "	Japanese Cryptomeria	Cryptomeria japonica
" " "	California Black Walnut	Juglans hindsii
113 D Street	Maidenhair Tree	Ginkgo biloba
114 E Street	Douglas Fir	Pseudotsuga menziesii
" " "	London Plane Tree	Platanus acerifolia
205 E Street	Lemon	Citrus limon
333 E Street	Myrtle (shrub)	Myrtus communis
" " "	Coast Redwood	Sequoia sempervirens
327 E Street	Valley Oak	Quercus lobata
325 E Street	Myrtle (shrub)	Myrtus communis
330 E Street	Siberian Elm	Ulmus pumila
305 F Street	Toyon (shrub)	Heteromeles arbutifolia
313 F Street	Siberian Elm	Ulmus pumila
329 F Street	Bridal-veil Broom	Genista monosperma
408 F Street	Grecian Laurel	Laurus nobilis
" " "	Incense Cedar	Libocedrus decurrens
416 F Street	Japanese Zelkova	Zelkova serrata
429 F Street	2 California Black Walnut	Juglans hindsii
433 F Street	English Holly	Ilex aquifolium
137 G Street	Tree-of-heaven	Ailanthus altissima
705 First Street	Tree-of-heaven	Ailanthus altissima
" " "	Deodar Cedar	Cedrus deodara
" " "	Valley Oak	Quercus lobata
" " "	Glossy privet	Ligustrum lucidum
Parking Lot #2, G Street	2 Black Locust	Robinia pseudoacacia

LOCATION	COMMON NAME	SCIENTIFIC NAME
403 G Street	Southern Magnolia	Magnolia grandiflora
403 G Street	2 Coast Redwood	Sequoia sempervirens
417 G Street	White Mulberry	Morus alba
" " "	California Black Walnut	Juglans hindsii
427 G Street	Coast Redwood	Sequoia sempervirens
" " "	Orange	Citrus sinensis
216 I Street	California Black Walnut	Juglans hindsii
220 E Street	2 London Planetree	Platanus acerifolia
224 I Street	Incense Cedar	Calocedrus decurrens
334 I Street	Colorado Spruce	Picea pungens
" " "	2 Valley Oak	Quercus lobata
419 I Street	Valley Oak	Quercus lobata
436 I Street	Deodar Cedar	Cedrus deodara
407 I Street	Camphor Tree	Cinnamomum camphora
315 I Street	California Black Walnut	Juglans hindsii
320 I Street	4 Orange	Citrus sinensis
327 I Street	Chinese Hackberry	Celtis sinensis
209 J Street	Deodar Cedar	Cedrus deodara
213 J Street	Pecan	Carya illinoensis
234 J Street	Silver Maple	Acer saccharinum
" " "	Deodar Cedar	Cedrus deodara
230 J Street	Incense Cedar	Calocedrus decurrens
300 J Street	Italian Cypress	Cupressus sempervirens
407 J Street	Coast Redwood	Sequoia sempervirens
411 J Street	Chinese Hackberry	Celtis sinensis
414 J Street	Valley Oak	Quercus lobata
421J Street	Chinese Hackberry	Celtis sinensis
" " "	English Walnut	Juglans regia
425 J Street	Chinese Hackberry	Celtis sinensis
437 J Street	Digger Pine	Pinus sabiniana
" " "	English Walnut	Juglans regia
434 J Street	2 Italian Cypress	Cupressus sempervirens
531 J Street	Chile Mavten	Maytenus boaria
217 K Street	California Black Walnut	Juglans hindsii
" " "	Blackwood Acacia	Acacia melanoxylon
231 K Street	2 Blackwood Acacia	Acacia melanoxylon
228 K Street	Blackwood Acacia	Acacia melanoxylon
232 K Street	California Redbud	Cercis occidentalis
336 K Street	California Black Walnut	Juglans hindsii
415 K Street	Valley Oak	Quercus lobata
419 K Street	European Olive	Olea europaea
341 K Street	2 European Olive	Olea europaea
337 K Street	European Olive	Olea europaea
420 K Street	London Planetree	Platanus acerifolia
426 K Street	2 London Planetree	Platanus acerifolia
431 K Street	2 English Walnut	Juglans regia
518 K Street	Red Pine	Pinus resinosa
1121 4thStreet	Chinese Privet	Ligustron sinense
334 I Street	Italian Cypress	Cupressus sempervirens
647 G Street	Platanus acerifolia	London Planetree
631 G Street	Platanus acerifolia	London Planetree
613 G Street	Robinia pseudoacacia	Black Locust
607 G Street	Juglans hindsii	California Black Walnut
536 G Street	3 Cedrus deodara	Deodar Cedar

LOCATION	COMMON NAME	SCIENTIFIC NAME
526 G Street	Platanus acerifolia	London Planetree
523 G Street	Betula verrucosa	European White Birch
525 G Street	Cedrus deodara	Deodar Cedar
515 G Street	2 Calocedrus decurrens	Incense Cedar
" " "	Fraxinus velutina	'Modesto' Modesto Ash
" " "	Prunus amygdalus	Almond
Union 76 Station 5th & G	Sequoia sempervirens	Coast Redwood
646 F Street	2 Celtis sinensis	Chinese Hackberry
636 F Street	Ulmus pumila	Siberian Elm
" " "	Cupressus sempervirens	Italian Cypress
620 F Street	Umbellularia californica	California Bay
" " "	Pinus ponderosa	Ponderosa Pine
618 F Street	Juglans hindsii	Black Walnut
" " "	Pinus halepensis	Allepo Pine
616 F Street	Sequoia sempervirens	Coast Redwood
508 F Street	Platanus acerifolia	London Planetree
522 F Street	2 Platanus acerifolia	London Planetree
" " "	Juglans regia	English Walnut
619 F Street	Laurus nobilis	Grecian Laurel
630 F Street	3 Juglans regia	English Walnut
643 F Street	Sequoia sempervirens	Coast Redwood
" " "	Ulmus pumila	Siberian Elm
637 F Street	Pinus nigra	Austrian Pine
633 F Street	Juglans regia	English Walnut
623 F Street	Juglans regia	English Walnut
613 F Street	Celtis sinensis	Chinese Hackberry
619 6th Street	Carya illinoensis	Pecan
613 6th Street	Fraxinus velutina	Arizona Ash

FIGURE 30
DESIGNATED HISTORICAL RESOURCES



CITY OF DAVIS DESIGNATED HISTORICAL RESOURCES

221 First Street - A.J. Plant House (Ord. 1343, 1/8/86) HRMC Minutes 11/26/85

Historical Resource AP #70-067-10
Zeta Psi Cal Davis Inc.
c/o Scott Diekman
373 Conway Drive
Danville, CA 95526

616 First Street - Boy Scout Cabin (Ord. 1282)

Historical Resource AP #70-260-01
UCD
c/o Davis Chamber of Commerce
228 B Street
Davis, CA 95616

209 Second Street - Barovetto Home (Ord. 1363, 4/9/86) HRMC Minutes 2/25/86

Historical Resource AP #70-064-04
Ursula H. Abbott
408 A Street
Davis, CA 95616

209 Second Street - Barovetto Tank House (Ord. 1363, 4/9/86)

Historical Resource AP #70-064-04
Ursula H. Abbott
408 A Street
Davis, CA 95616

505 Second Street - H.J. Hamel House (Ord. 291, 11/14/84)) HRMC Minutes 9/25/84

Historical Resource AP #70-241-06
Bill D./ Nanelle Arnold
c/o Arnold Property Mgmt.
425 Second Street
Davis, CA 95616

*604 Second Street - Dresbach-Hunt-Boyer Home (Ord. 1282, 7/25/84) HRMC Minutes 4/24 &
5/29/84

Outstanding AP #70-243-02
Historical Resource Lawrence/Nancy Shepard
c/o Pat Moore
1008 Vassar Drive
Davis, CA 95616

713, 715, 719 Second Street - See 203 G Street

Historical Resource AP #70-251-08
BRB Investors
c/o The Bookkeeper
603 G Street
Davis, CA 95616

Davis Designated Historical Resources

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716, 718, 720, 722, 724, 726 Second Street - Brinley Block (Ord. 1291)

Historical Resource AP #70-254-06 and 01
John W./ Laurette Brinley
Brinley Trust
209-A F Street
Davis, CA 95616

*840 Second Street - Southern Pacific Station/ Davis Junction (Ord. 1282)

Outstanding AP #SBE 872-57-1C-13
Historical Resource Southern Pacific Trans. Co.
One Market Plaza Room 6901
S.P. Building
San Francisco, CA 94105

232 Third Street - Eggleston Home (Ord. 1410, 1/7/87) HRMC Minutes 11/25/86

Historical Resource AP #70-065-01
Dorothy M. Pugh
909 Sonoma Way
Sacramento, CA 95819

619 Fourth Street - First Presbyterian Manse (Ord. 1295)

Historical Resource AP #70-213-14
Rodney A. Lane
619 Fourth Street
Davis, CA 95616

623 Seventh Street - Anderson-Hamel House (Ord. 1355, 2/19/86) HRMC Minutes 11/26/85

Historical Resource AP #70-165-18
Dr. L.H. Storm
58 College Park
Davis, CA 95616

310 A Street - Asbill-Grieve House (Ord. 1364, 4/9/86) HRMC Minutes 2/25/86

Historical Resource AP #70-074-07
Richard & Elizabeth Berteaux
2208 Alameda
Davis, CA 95616

232 B Street - Jacobson-Wilson House (Ord. 1295, 11/28/84) HRMC Minutes 10/23/84

Historical Resource AP #70-231-04
Carolyn S. Shine
232 B Street
Davis, CA 95616

Davis Designated Historical Resources

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337 B Street - McDonald House (Ord. 1360, 3/12/86) HRMC Minutes 1/28/86

Historical Resource AP #70-073-17
Scott E. Gordon
325 Howard Avenue
Piedmont, CA 94611

137 C Street - Clancy House (Ord. 1334, 12/4/85) HRMC Minutes 6/25 & 9/24/85

Historical Resource AP #70-234-06
Alpha Chi Omega
137 C Street
Davis, CA 95616

*412 C Street - Davis Community Church (Ord. 1282)

Outstanding AP #70-202-01
Historical Resource Davis Community Church
412 C Street
Davis, CA 95616

445 C Street - Old Davis Library (Ord. 1282)

Historical Resource AP #70-243-07
In computer as: #70-201-01
City of Davis
23 Russel Blvd.
Davis, CA 95616

*226 F Street - Old Davis City Hall (Ord. 1282)

Outstanding AP #70-251-01
Historical Resource City of Davis
23 Russell Blvd.
Davis, CA 95616

*203 G Street - Anderson Bank Building (Ord. 1282)

Outstanding Same as 713-719 Second Street
Historical Resource AP #70-251-08
BRB Investors
603 G Street
Davis, CA 95616

225 G Street - Masonic Lodge (Ord. 1291)

Historical Resource AP #70-251-13
Freddy Fast Pants Inc.
1855 41st Avenue, D-8
Capitola, CA 95010

301 G Street - Bank of Yolo (Ord. 1291)

Historical Resource AP #70-216-10
Davis Enterprise
P.O. Box 1078
Davis, CA 95617

Davis Designated Historical Resources

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*320 I Street - Williams -Drummond-Rorvick House (Ord. 1282)

Outstanding AP #70-323-03
Historical Resource Kenneth C. Gebhart
7781 N. Fancher Avenue
Clovis, CA 93612

*334 I Street - Schmeiser House (Ord. 1335, 12/4/85) HRMC Minutes 6/25 & 9/24/85

Outstanding AP #70-323-01
Historical Resource Donald & Judith Brooks
334 I Street
Davis, CA 95616

405 J Street - McBride Home (Ord. 1402, 12/3/86) HRMC Minutes 9/23 & 10/28/86

Historical Resource AP #70-322-08
Dan L. Brown
405-A J Street
Davis, CA 95616

*434 J Street - Joshua B. Tufts-Longview-Jones Home (Ord. 1282)

Outstanding AP #70-331-01
Historical Resource Valerie Jones
434 J Street
Davis, CA 95616

*1140 Los Robles - Werner-Hamel House (Ord. 1282)

Outstanding AP#69-230-05
Historical Resource Arnold & Jean Sillman
1140 Los Robles
Davis, CA 95616

*820 Pole Line Rd. - Davis Cemetery (Ord. 1282)

Outstanding AP #32-570-01
Historical Resource Yolo County
c/o Davis Cemetery
820 Pole Line Road
Davis, CA 95616

*Russell Boulevard, West of Arthur Street - Avenue of the Trees (Ord. 1282)

Outstanding
Historical Resource

*23 Russell Boulevard - Davis City Offices (Ord. 1282)

Outstanding AP #70-083-03
Historical Resource City of Davis
23 Russell Blvd.
Davis, CA 95616

*2020 Russell Boulevard - LaRue-Romard Home (Ord.1282)

Outstanding AP #36-142-07
Historical Resource N/A

"Historical resource" shall mean buildings, structures, signs, features, sites, places, areas, or other improvements of scientific, aesthetic, educational, cultural, archaeological, architectural, or historical value to citizens of the city of Davis and designated as such by the City Council pursuant to the provisions of this chapter. When application is made for an alteration permit to demolish a historical resource, the commission's disapproval of the permit means demolition can be suspended for up to 360 days pursuant to section 29.145 (14) (d) of the Historical Resources Management Ordinance.

* "Outstanding historical resource" shall mean buildings, structures, signs, features, sites, places, areas, or other improvements of the highest scientific, aesthetic, educational, cultural, archaeological, architectural, or historical value to citizens of the city of Davis and designated as such by the City Council pursuant to the provisions of this article. An outstanding historic resource is deemed to be so important to the historical and architectural fabric of the city that its loss would be a major loss to the city. When application is made for an alteration permit to demolish an outstanding historical resource, the commission's disapproval of the application means that no alteration permit shall be issued (section 29.145 (14)(d) of the Historical Resources Management Ordinance).